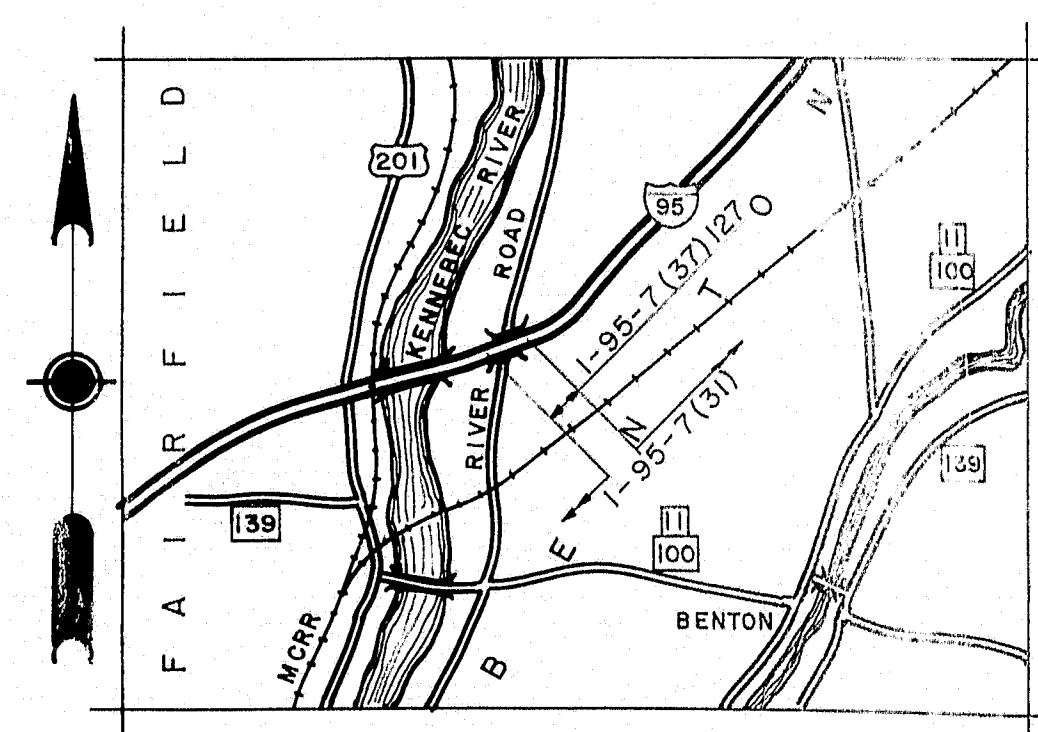


STATE OF MAINE
STATE HIGHWAY COMMISSION



INTERSTATE 95
OVER
RIVER ROAD
IN THE TOWN OF
BENTON
KENNEBEC COUNTY

FEDERAL AID PROJECT NO. I-95-7(37)127
LENGTH OF PROJECT 0.074 MILES



LOCATION MAP
APPROX. SCALE - 1" = 1 MILE

INDEX OF SHEETS

1. TITLE SHEET
 2. FOUNDATION SURVEY
 - 3, 4. BORING DETAILS
 5. ROADWAY WORK, PROFILE
 - 6, 7. CROSS SECTIONS, S.B.
 8. CROSS SECTIONS, N.B.
 9. CROSS SECTIONS, N.B. - TYPICAL SECTIONS
 10. GENERAL PLAN & QUANTITIES
 11. ABUTMENT NO. 1, S.B.
 12. ABUTMENT NO. 2, S.B.
 13. ABUT. PILE PLANS, S.B.
 14. ABUTMENT NO. 1, N.B.
 15. ABUTMENT NO. 2, N.B.
 16. APPROACH SLABS, N.B. & S.B.
 17. PIERS, S.B.
 18. PIERS, N.B.
 19. SLOPE PAVING DETAILS, N.B. & S.B.
 20. STRUCTURAL STEEL, S.B.
 21. STRUCTURAL STEEL, N.B.
 22. ARMORED JOINTS & DRAIN DETAILS S.B. & N.B.
 23. BLOCKING, S.B. & N.B.
 - 24, 25. SUPERSTRUCTURE, S.B.
 - 26, 27. SUPERSTRUCTURE, N.B.
 28. REINFORCING STEEL SCHEDULE, S.B.
 29. REINFORCING STEEL SCHEDULE, N.B.
- STANDARDS - BD 101-62, BEARING PEDESTALS
BD 102-62, BRIDGE RAIL
BD 103-62, BEAM SPLICES

SCALES

PLAN & PROFILE { HOR. 1" = 20'
VERT. 1" = 10'
CROSS SECTIONS 1" = 10'

TRAFFIC

INTERSTATE 95	RIVER ROAD
6535	A.D.T. 1962 400
8880	A.D.T. 1982 560
1066	D.H.V. 56
11%	T 8%
60%	D 60%
60 MPH	V

APPROVED
MAINE STATE HIGHWAY COMMISSION

David H. Hines
CHAIRMAN
Edward J. Hines
John Williams
Raymond J. Hines
CHIEF ENGINEER

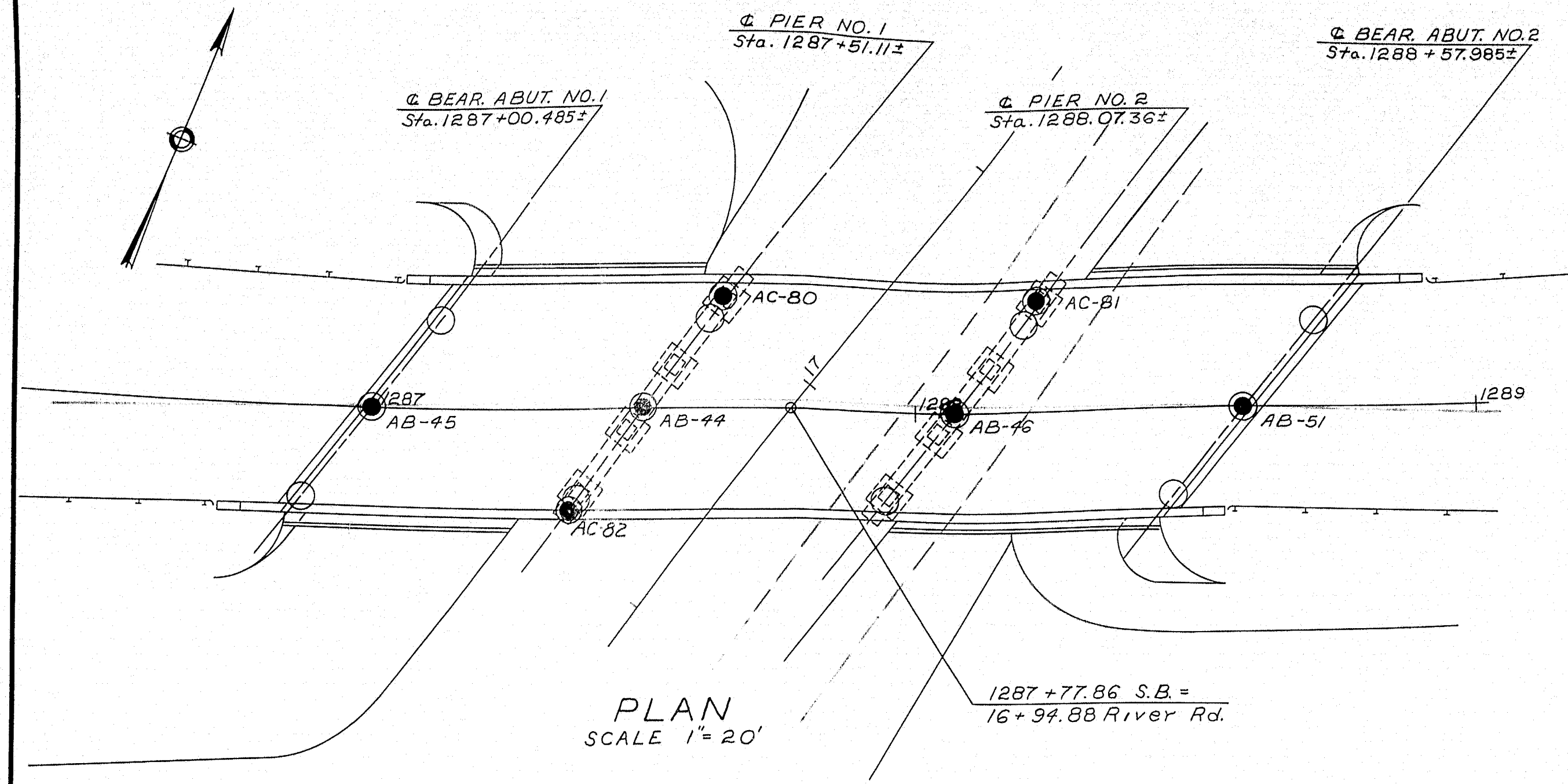
4-10-63
DATE

DEPARTMENT OF COMMERCE
BUREAU OF PUBLIC ROADS
REGION 1

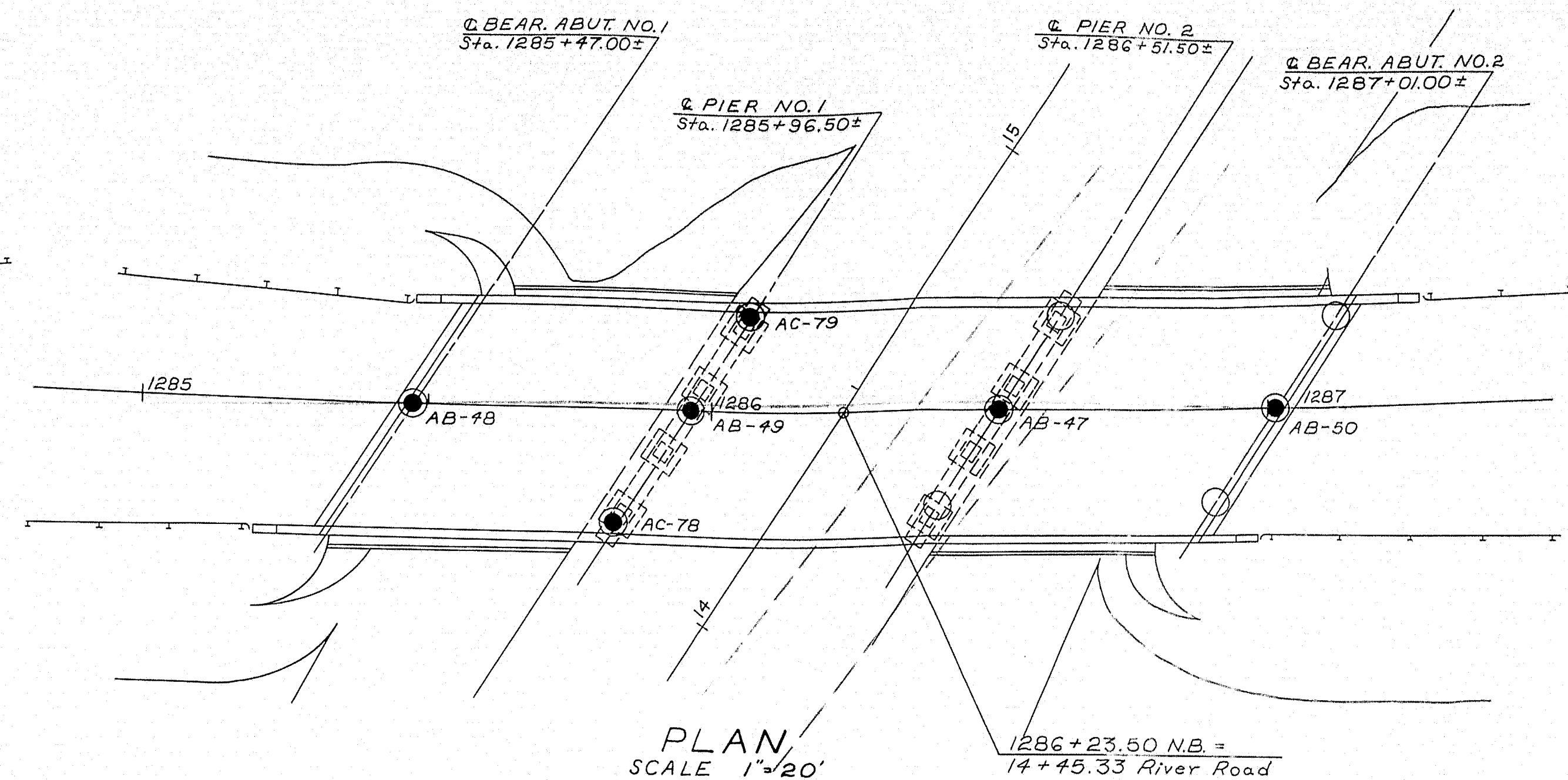
APPROVED

DIVISION ENGINEER DATE

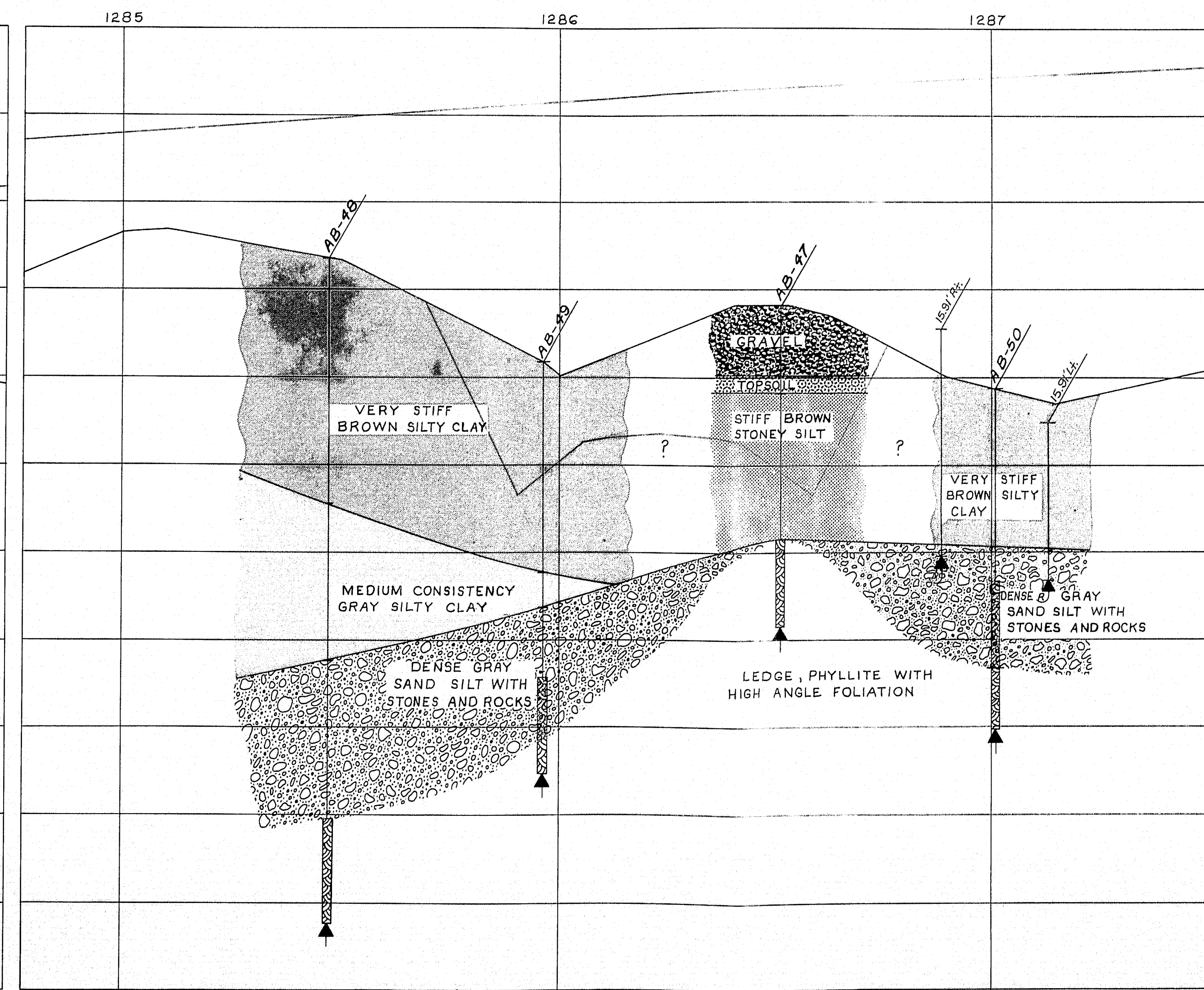
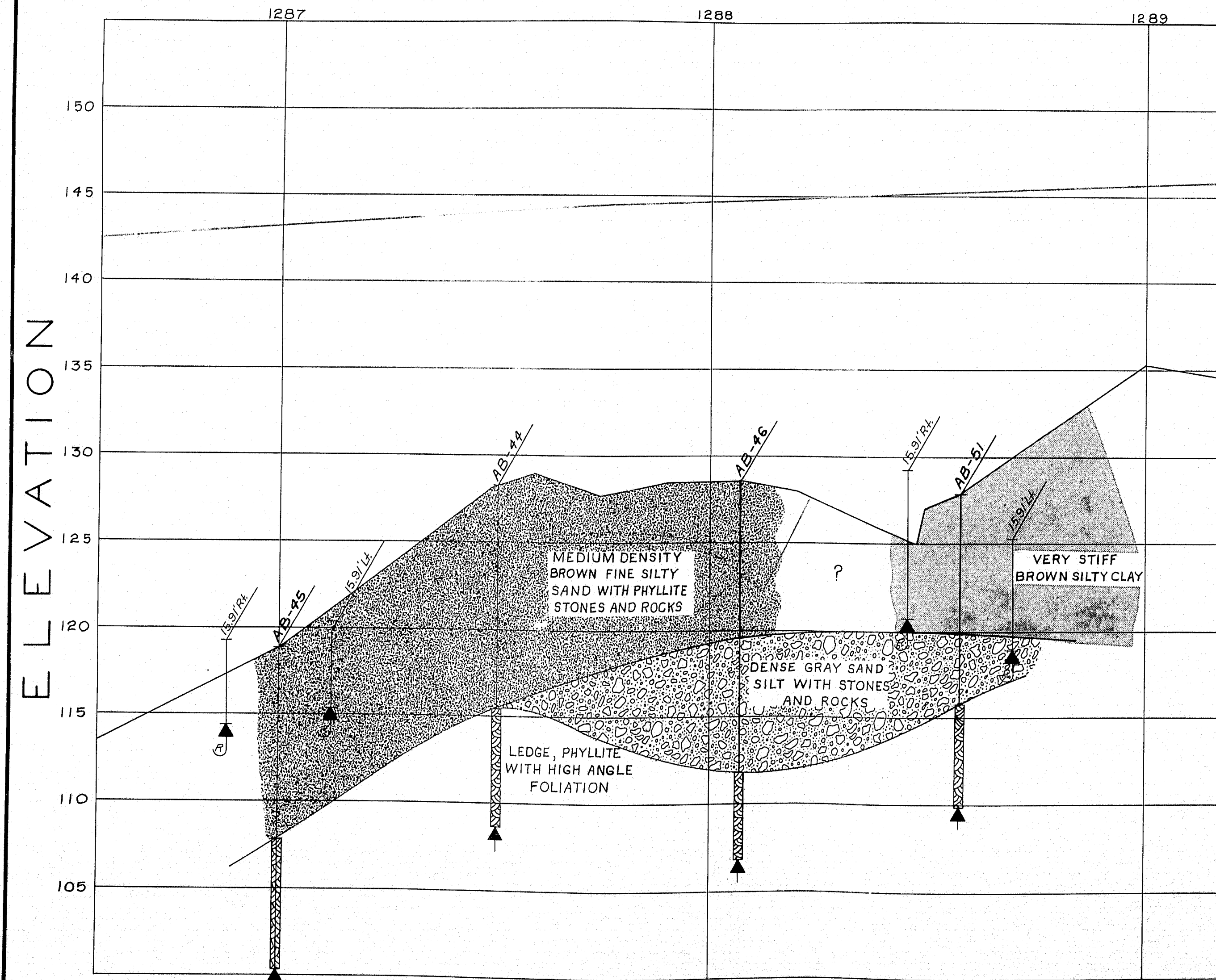
D. P. R. REG. NO.	STATE	PROJECT NUMBER	SHEET NO.	TOTAL SHEETS
1	MAINE	I-95-7(37)	2	29



SOUTHBOUND LANE



NORTHBOUND LANE



DESIGN—
TRACE—
CHECK—

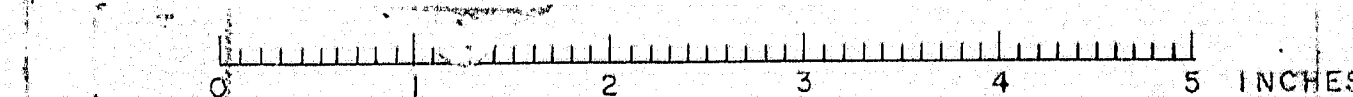
BRIDGE NO.
SURVEY—
PLOT—

STATE HIGHWAY COMMISSION
BRIDGE DIVISION

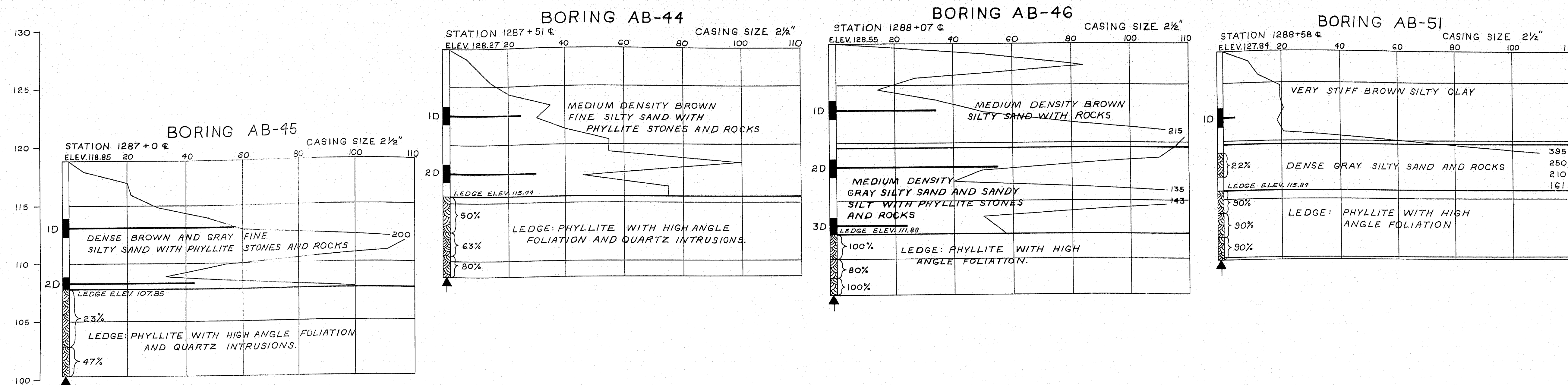
INTERSTATE 95
OVER
RIVER ROAD
IN THE TOWN OF
BENTON
KENNEBEC COUNTY
FOUNDATION SURVEY

SHEET 2 OF 29 AUGUSTA, MAINE JUNE 1963

70-2



SOUTHBOUND LANE



BORING NOTES

ALL SAMPLES AND VANES ARE MADE AHEAD OF CASING.

NUMBER OF BLOWS REQUIRED TO DRIVE EXTRA HEAVY CASING ONE FOOT WITH 400 FT. LBS. OF ENERGY PER BLOW.

LOCATION OF SAMPLE OR SAMPLE ATTEMPT. NUMBER AND TYPE OF DRY SAMPLE.

S&H SAMPLER #1290'S

2" O.D. 16 GA. SEAMLESS TUBING

UNSUCCESSFUL SAMPLE ATTEMPT AND TYPE OF SAMPLER.

NUMBER OF BLOWS REQUIRED TO DRIVE SPOON OR TUBING ONE FOOT WITH 350 FT. LBS. OF ENERGY PER BLOW.

SAMPLING SPOON OR SEAMLESS TUBING DRIVEN BY STATIC WEIGHT OF DRILL RODS AND HAMMER.

FIELD VANE TEST

BOTTOM OF BORING (MAY NOT BE BOTTOM OF SOIL STRATA)

LOCATIONS CORED BY DIAMOND BIT AND PER CENT RECOVERY OF ROCK.

SHEAR NOTES

• FIELD VANE SHEAR STRENGTHS

x LABORATORY VANE SHEAR STRENGTHS

→ SHEAR STRENGTHS IN EXCESS OF CAPACITY OF EQUIPMENT

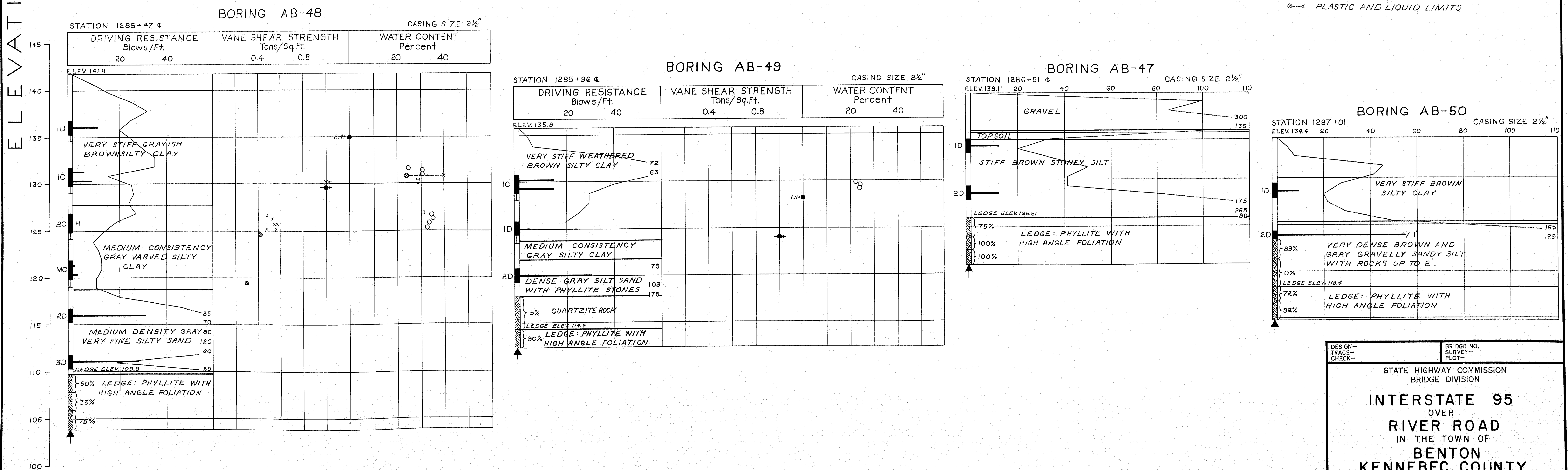
○ ONE HALF UNCONFINED COMPRESSIVE STRENGTHS.

WATER CONTENT NOTES

○ NATURAL WATER CONTENTS, GIVEN AS PER CENT OF DRY WEIGHT.

○-x PLASTIC AND LIQUID LIMITS

NORTHBOUND LANE



DESIGN-
TRACE-
CHECK-

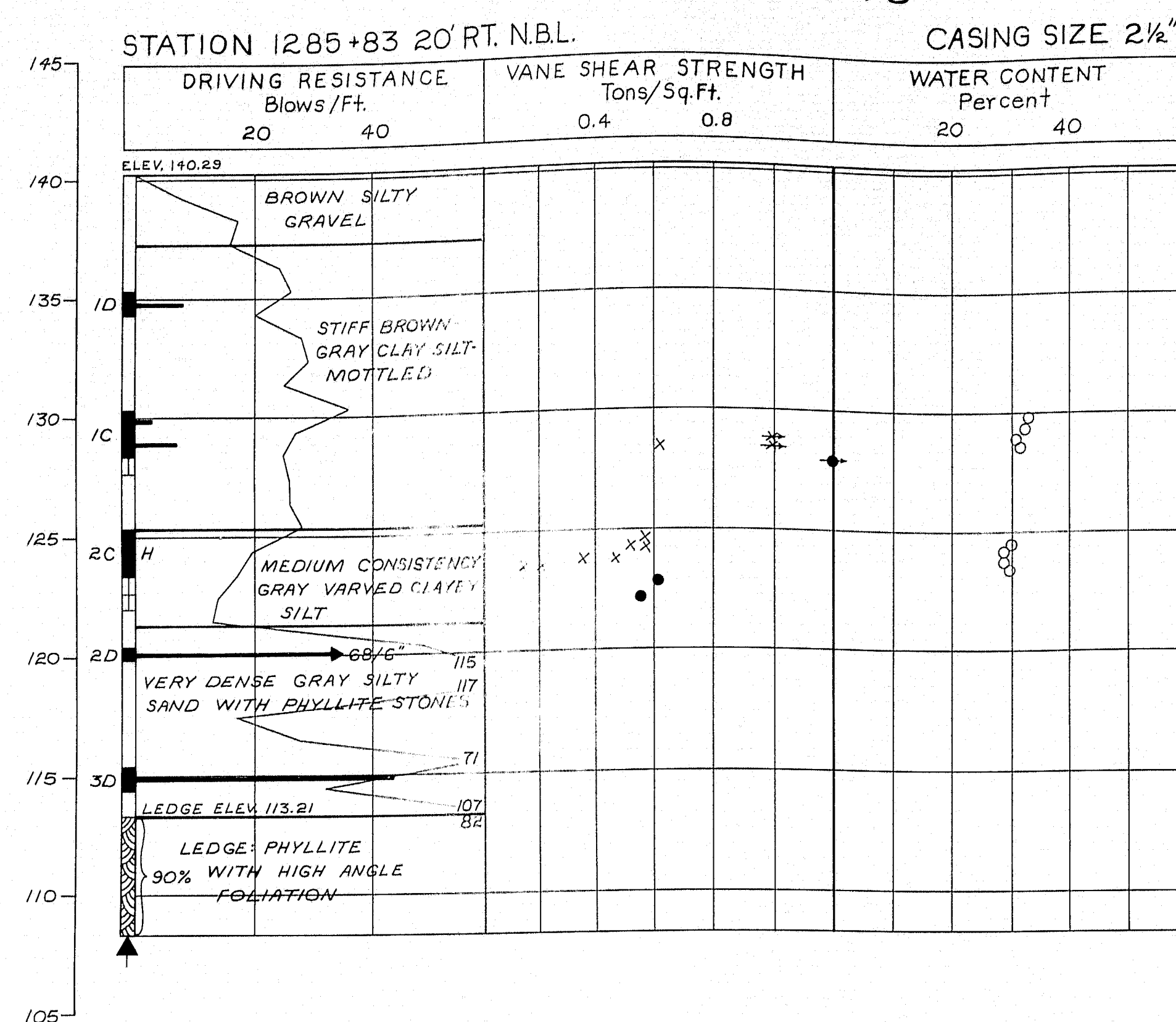
BRIDGE NO.
SURVEY-
PLOT-

STATE HIGHWAY COMMISSION
BRIDGE DIVISION

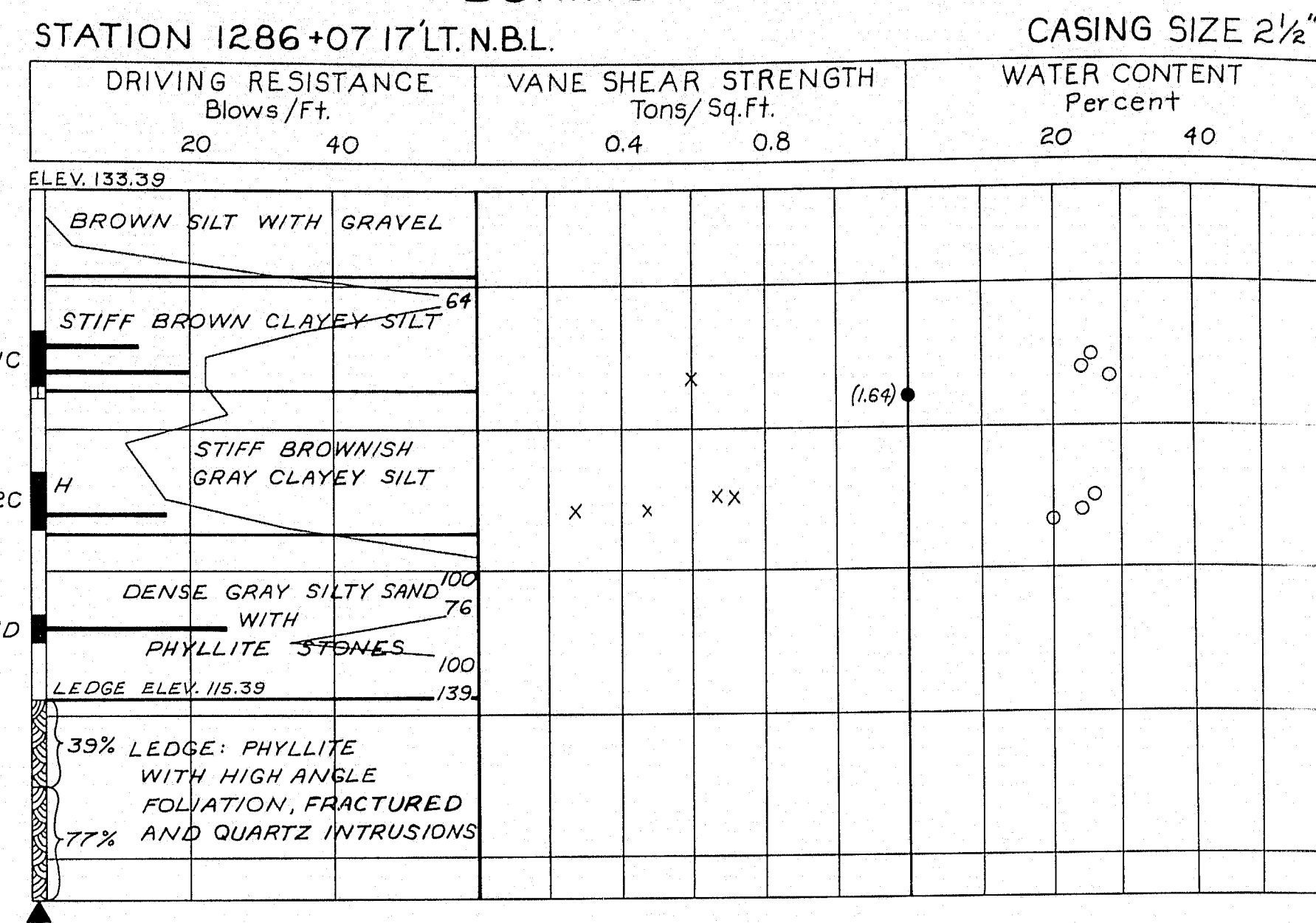
INTERSTATE 95
OVER
RIVER ROAD
IN THE TOWN OF
BENTON
KENNEBEC COUNTY
BORING DETAILS

SHEET 3 OF 29 AUGUSTA, MAINE JUNE 1963

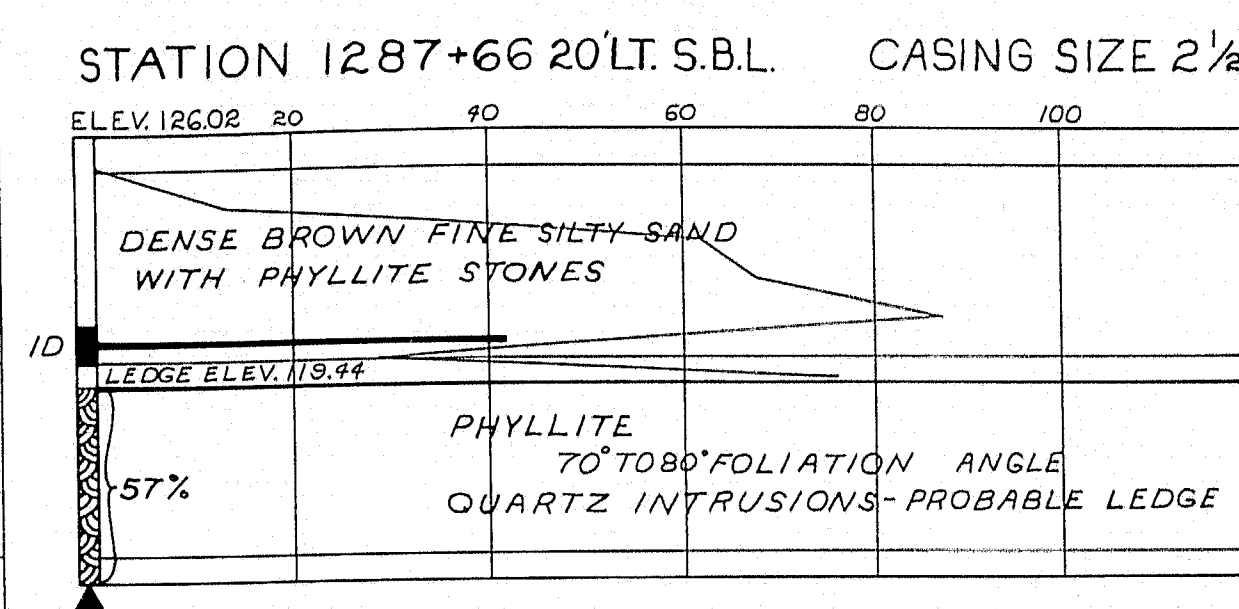
BORING NO. AC-78



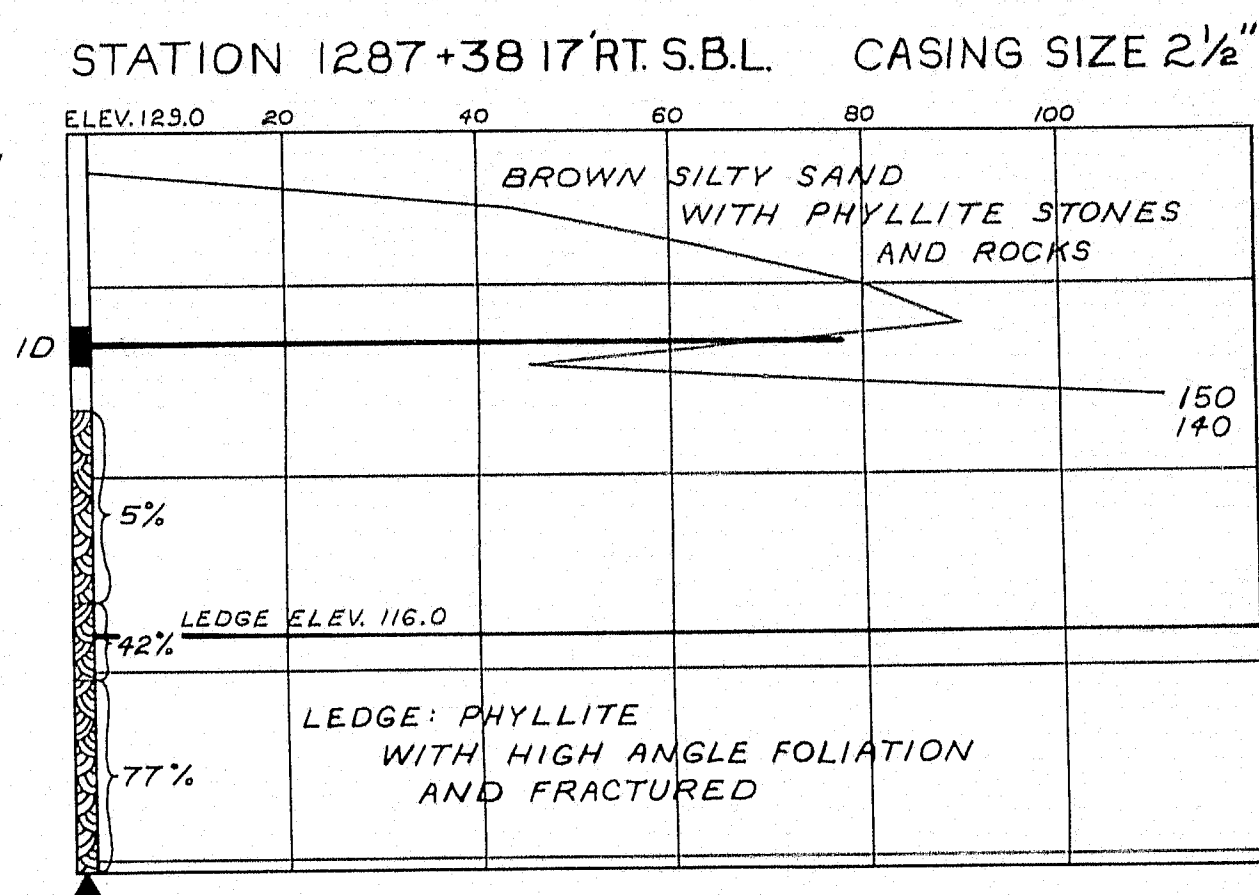
BORING NO. AC-79



BORING NO. AC-80

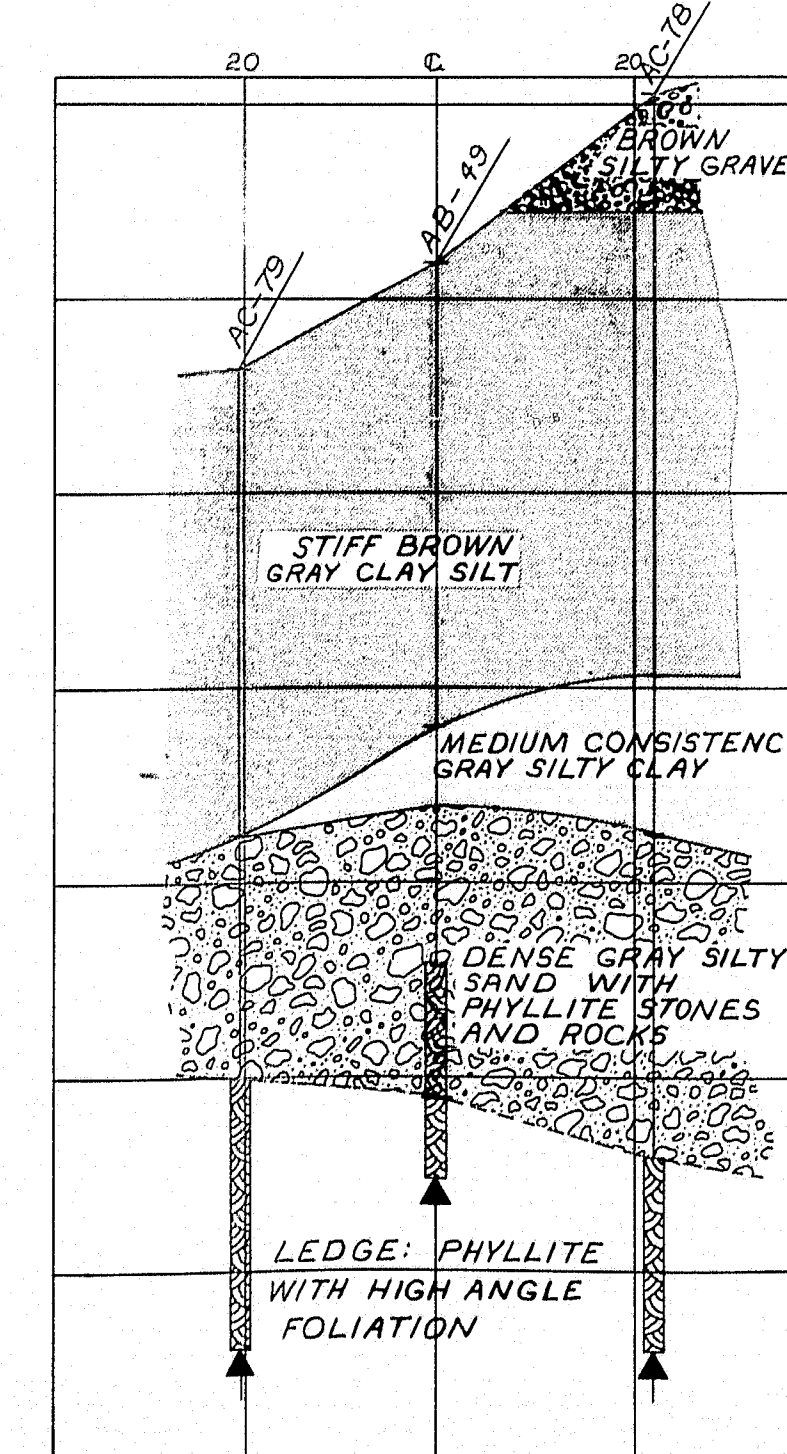


BORING NO. AC-82

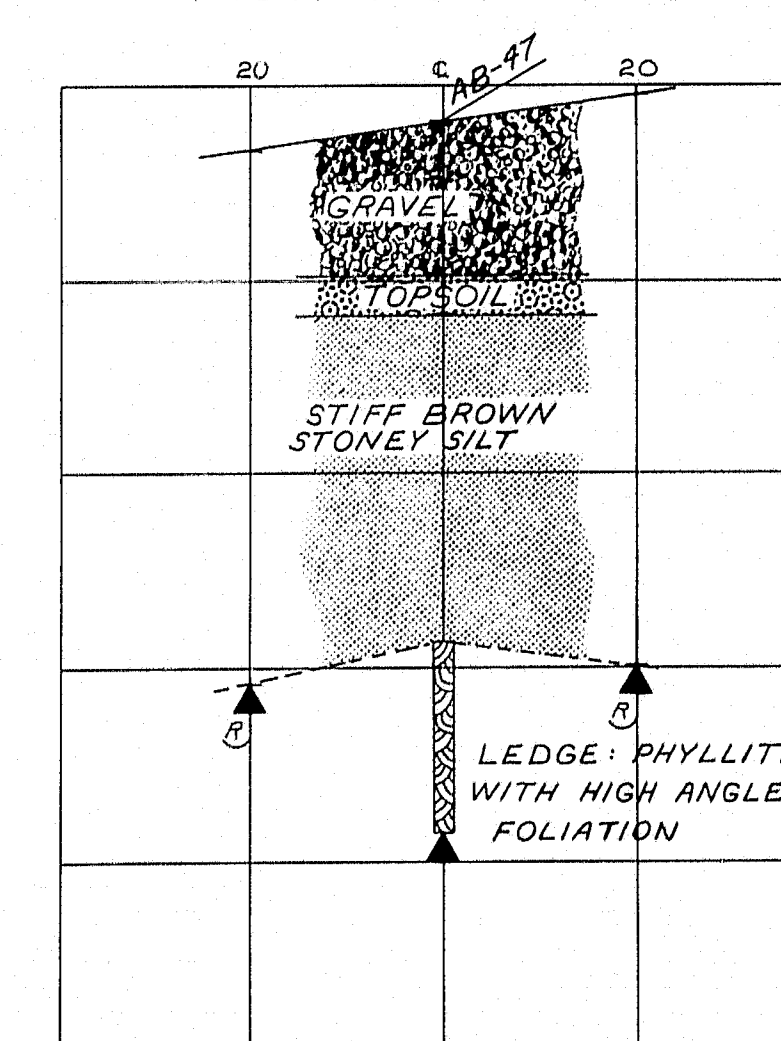


TRANSVERSE SECTIONS

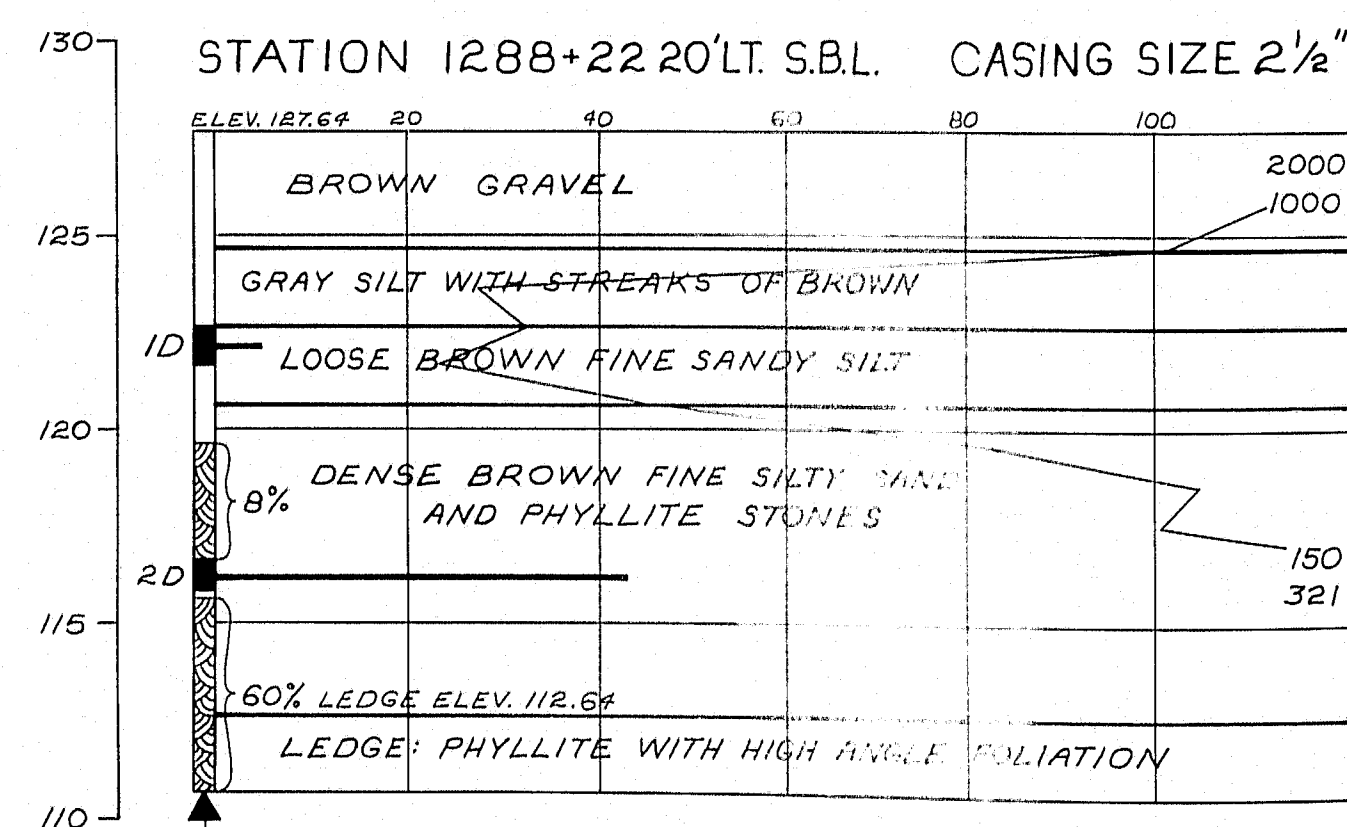
PIER NO. 1 N.B.L.



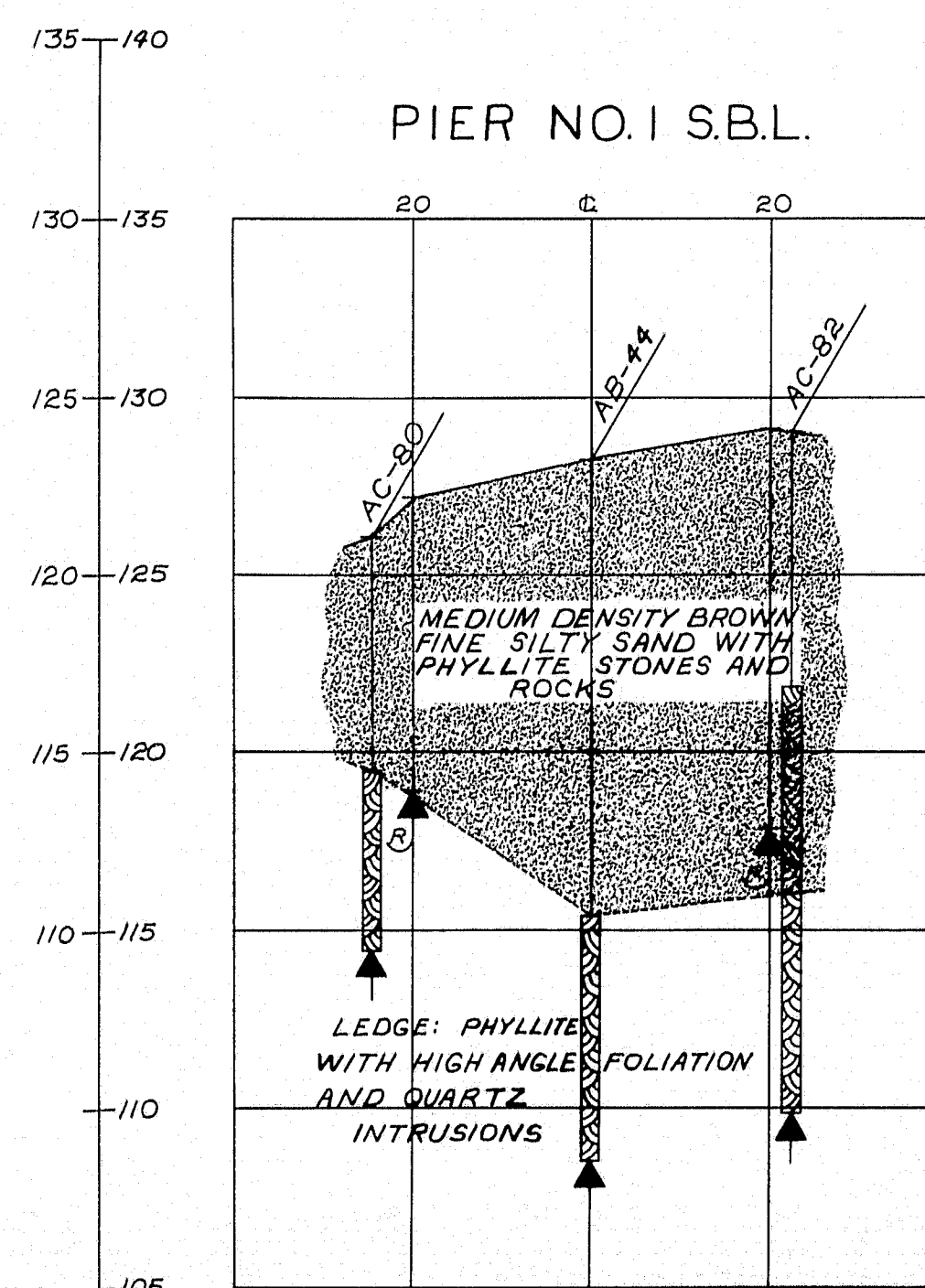
PIER NO. 2 N.B.L.



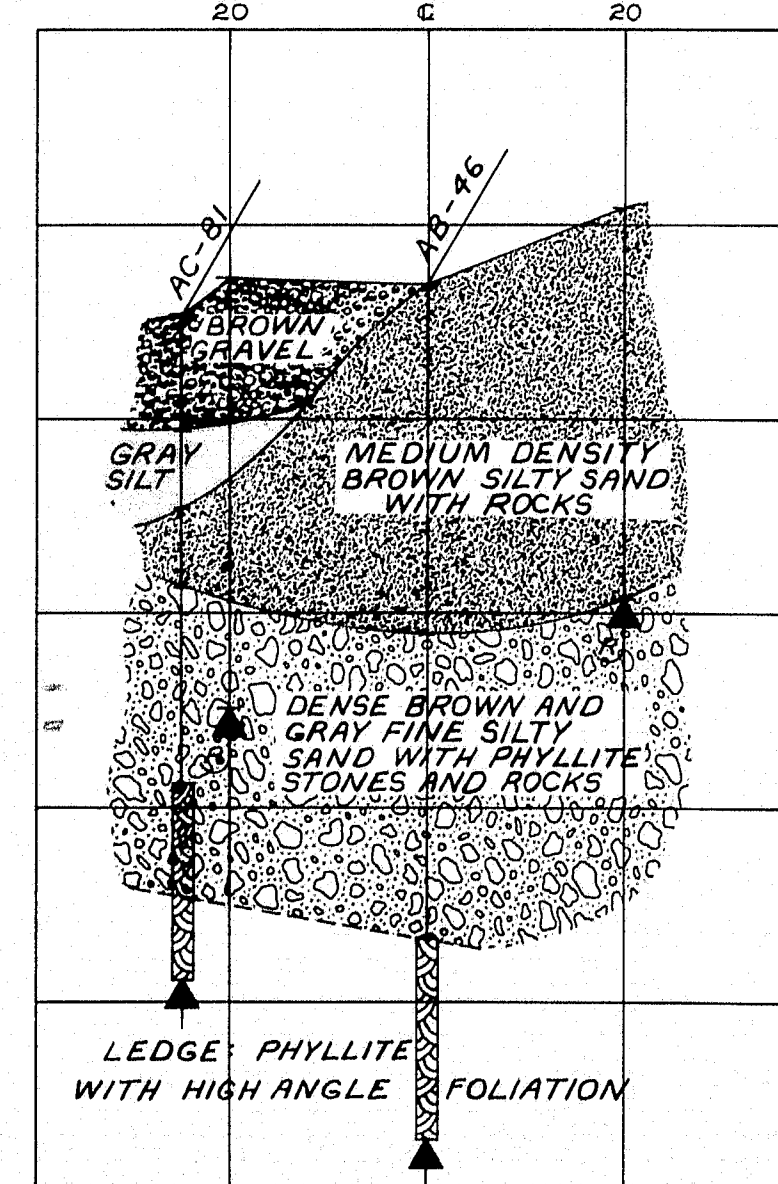
BORING NO. AC-81



PIER NO. 1 S.B.L.



PIER NO. 2 S.B.L.



DESIGN-CHECK
TRACE-
PLOT-

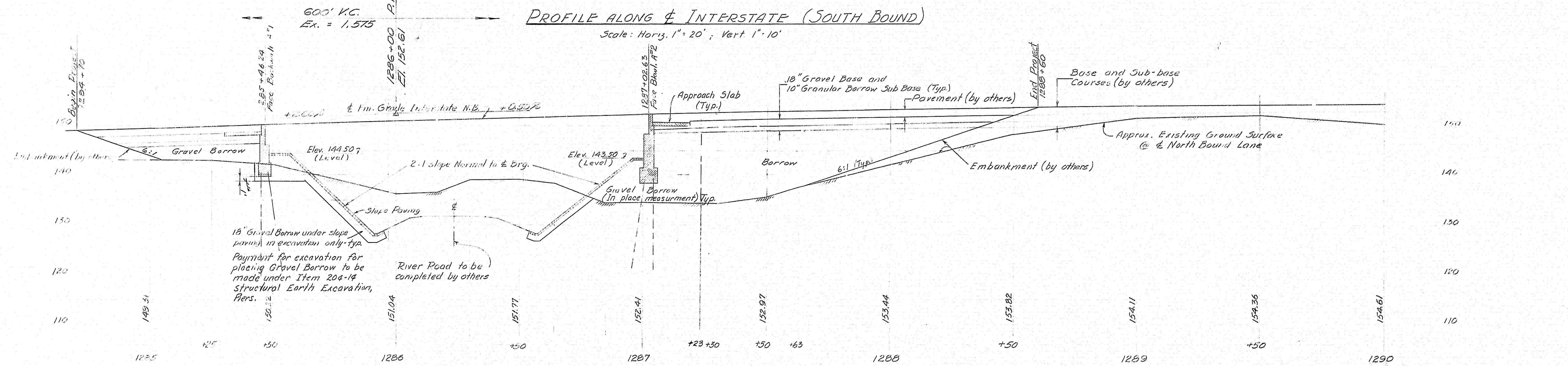
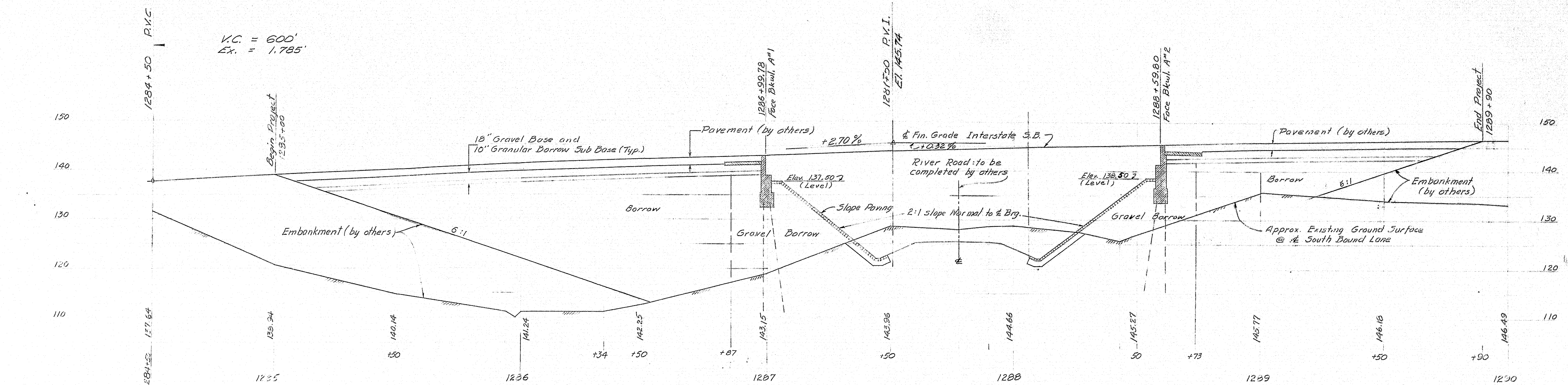
BRIDGE NO.
SURVEY-
PLOT-

STATE HIGHWAY COMMISSION
BRIDGE DIVISION

INTERSTATE 95
OVER
RIVER ROAD
IN THE TOWN OF
BENTON
KENNEBEC COUNTY
BORING DETAILS

SHEET 4 OF 29 AUGUSTA, MAINE JUNE 1963

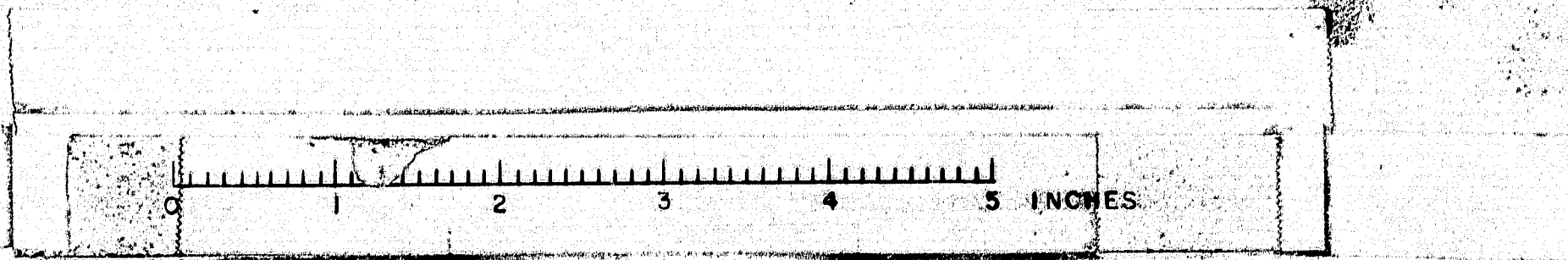
S.R. & S.	STATE	PROJECT NUMBER	SHEET NO.	TOTAL SHEETS
1	MAINE	1-95-7(17)	6	28



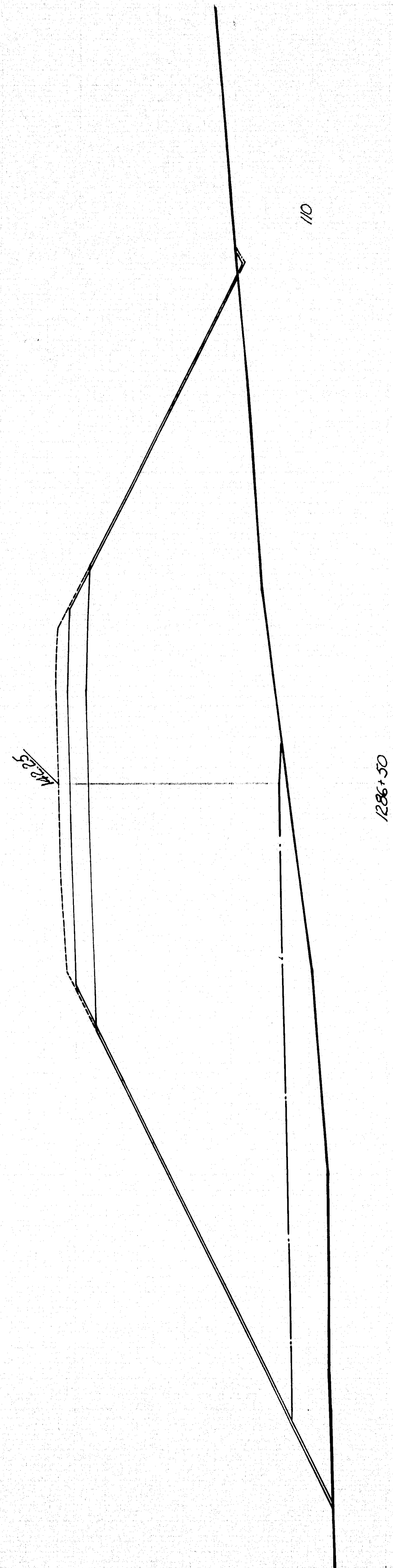
Note: As built revisions shown in black ink. 9-24-64 E. Barnard

- NOTES:**
- Place gravel borrow to bottom of abutment footings before driving piles.
 - Lawn, Seeding Method No. 2, Hay Mulch & Asphalt Mulch Binder up to bottom of Granular Borrow South Bound Sta. 1285+00 to 1289+90, North Bound Sta. 1284+50 to 1288+60 to be done under Bridge Contract; Elsewhere by others.
 - The vertical planes at the intersection of borrow and Gravel Borrow are shown parallel to & bearings of adjacent abutments.

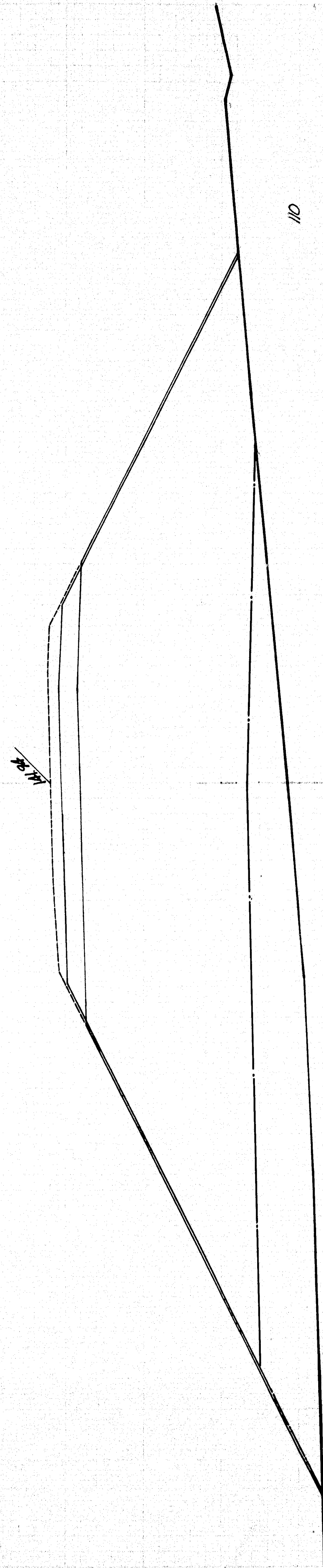
DESIGN - AMR HRD	DET - FBF	BRIDGE NO.
TRACE - FBF	CHECK - D. Barnard	PROJECT NO.
STATE HIGHWAY COMMISSION		
BRIDGE DIVISION		
INTERSTATE 95 OVER RIVER ROAD		
IN THE TOWN OF		
BENTON		
KENNEBEC COUNTY		
ROADWAY WORK, PROFILE		
SHEET 6 OF 29 AUGUSTA, MAINE JUNE 1965		



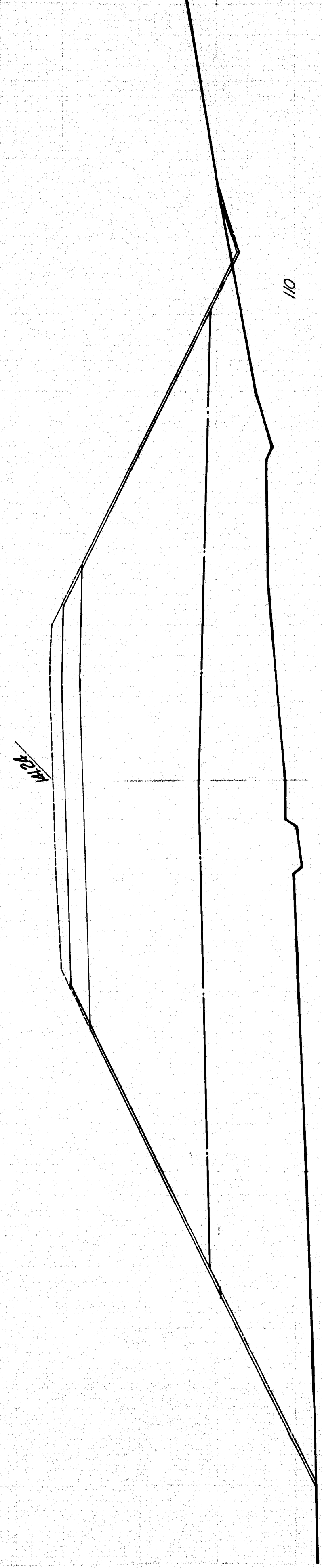
D. P. R. REG. NO.	STATE	PROJECT NUMBER	SHEET NO.	TOTAL SHEETS
1	MAINE	1-95-7(37)	6	29



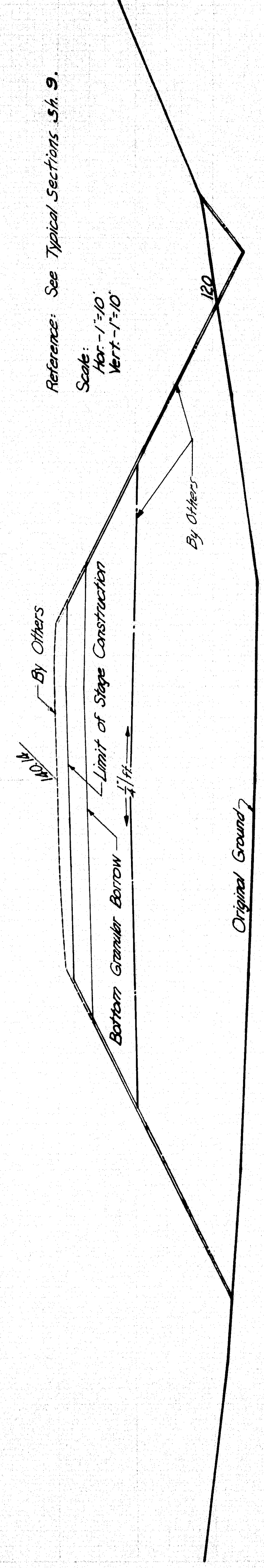
1286+50



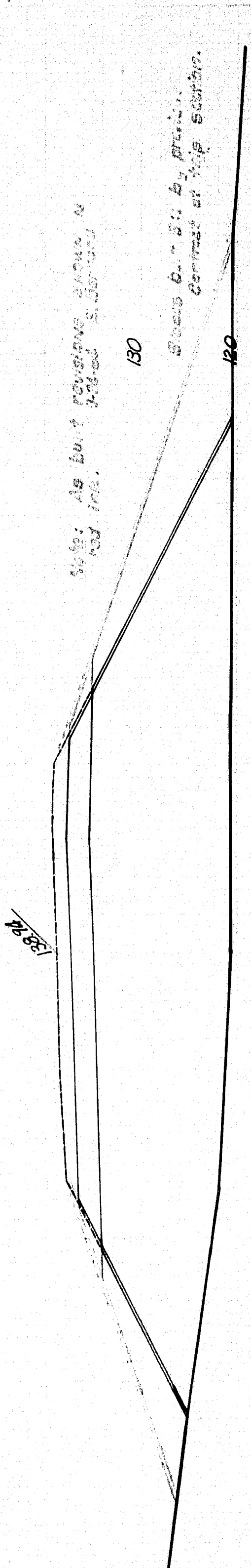
1286+34



1286+0



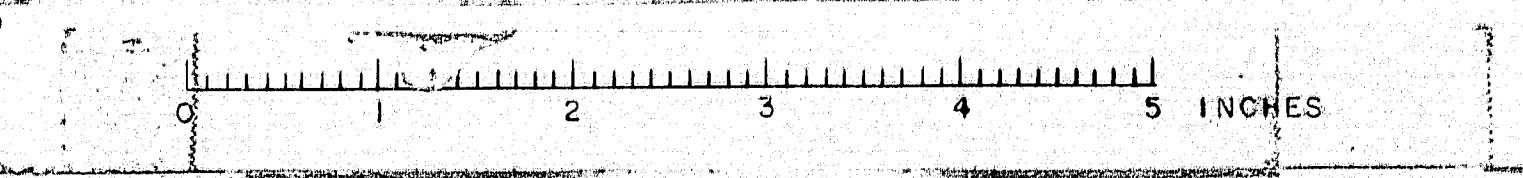
1286+50



1286+0

Reference: See Typical Sections Sh. 9.
Scale:
Hor. 1"=10'
Vert. 1"=10'

DETAIL - RGS TRACE - RGS CHECK - <i>Dressley</i>	STATE HIGHWAY COMMISSION BRIDGE DIVISION INTERSTATE 95 OVER RIVER ROAD IN THE TOWN OF BENTON KENNEBEC COUNTY CROSSECTIONS 88 SHEET 6 OF 29 AUGUSTA MAINE JUNE 1963
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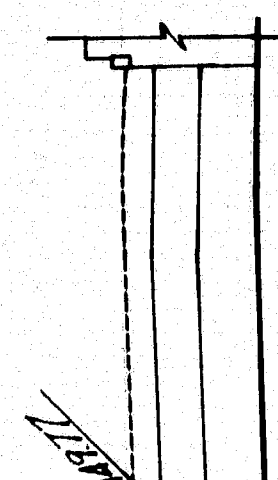
95/479

- BRIDGE -

1285+46.24 (N.B.)
Face of Backwall
About No. 1

1285+25

130



DETAIL - ROG	
TRACE - ROG	
CHECK - <i>Donnelly</i>	
STATE HIGHWAY COMMISSION BRIDGE DIVISION	
INTERSTATE 95 OVER RIVER ROAD	
IN THE TOWN OF BENTON KENNEBEC COUNTY	
CROSSECTIONS NB	
SHEET 8 OF 29 AUGUSTA MAINE 1963	

D. P. R. REG. NO.	STATE	PROJECT NUMBER	SHEET NO.	TOTAL SHEETS
1	MAINE	1-95-7 (37)	8	28

Original Ground 7

Arch 140
by others
(typical)

Limit of Stage Construction

1285+00

By Others

Gravel Barrow

Endowment
by others

Bottom Gravel Barrow

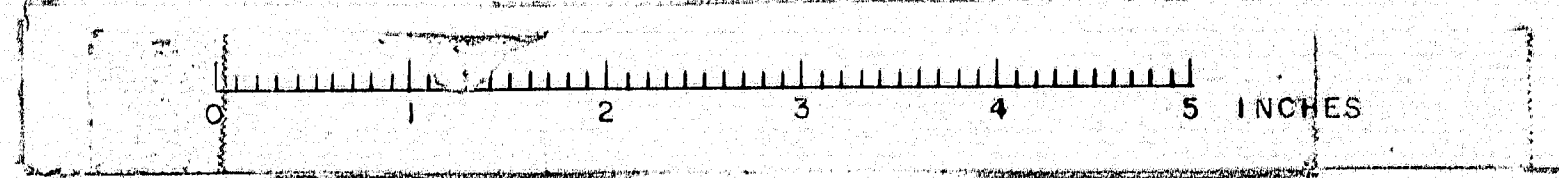
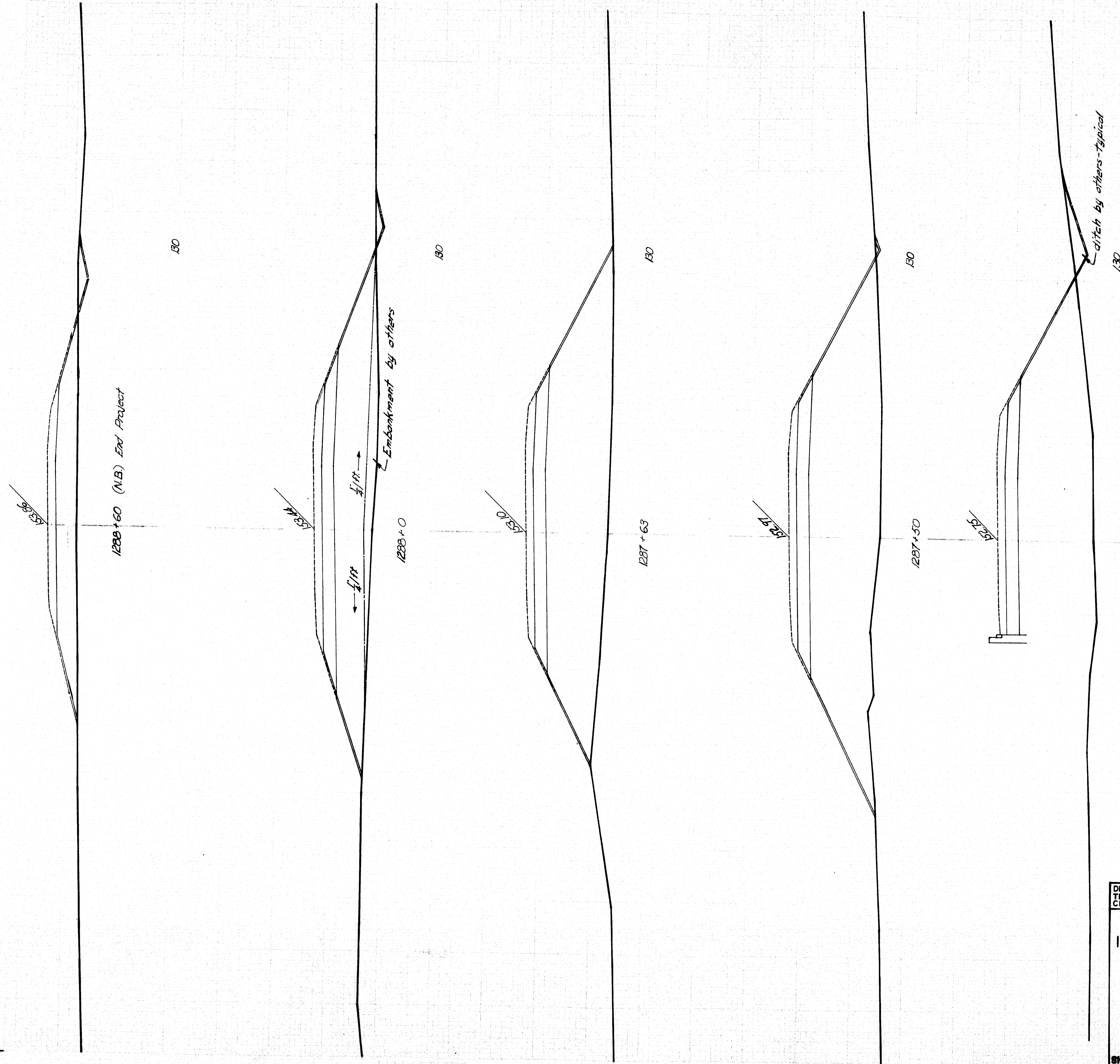
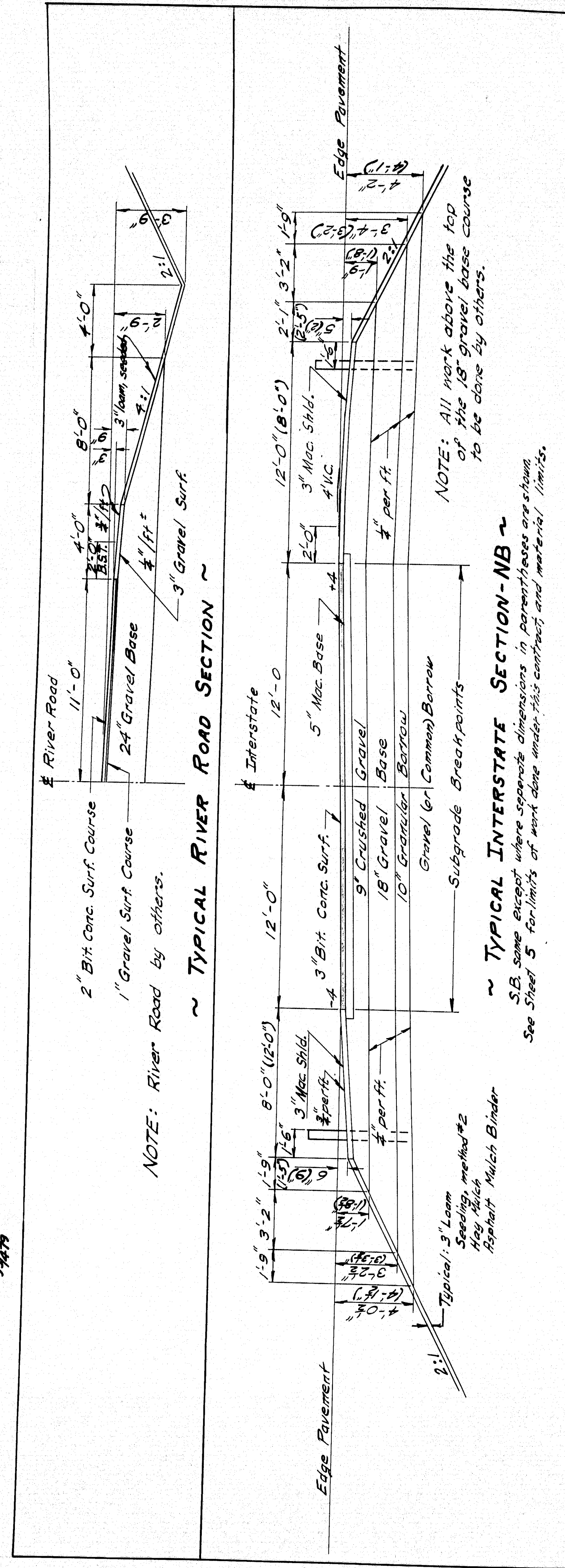
130 Reference: See Typical Sections Sh. 9.

Scale:
Hor. 1"=10'
Vert. 1"=10'

1284+70
Begin Project

4 (N.B.)

0 1 2 3 4 5 INCHES



CURVE DATA

Horizontal	Vertical
Southbound PVI Sta 1287+36.85 L = 40'-23'-26" LT D = 1°-18' T = 1621.18' L = 3106.97' R = 4407.37' E = 288.71'	PVI Sta 1287+50, El. 145.74 L = 600' E = 178.50' g ₁ = +2.70% g ₂ = +0.32% P.V.C. Sta 1284+50, El. 137.64

Horizontal	Vertical
Northbound PVI Sta 1283+49.84 L = 43'-07'-43" LT D = 1°-15' T = 1813.03' L = 3472.96' R = 4583.66' E = 345.54'	PVI Sta 1286+00, El. 152.61 L = 600' E = 157.50' g ₁ = +2.60% g ₂ = 0.60% P.V.C. Sta 1283+00, El. 144.81

River Road

Vertical
 PVI Sta 17+90, El. 117.48
 P.V.C. Sta 14+05, El. 133.73
 L = 650'
 E = 8.1230
 g₁ = -5.00%
 g₂ = +5.00%

UTILITIES

Central Maine Power Company
 New England Tel. & Tel. Company

ESTIMATE OF QUANTITIES

ITEM DESCRIPTION	UNIT	TOTAL QUANTITIES	BRIDGE QUANTITIES
Structural Earth Excavation-Abutments & Retaining Walls	Cu. Yds.	50	50
Structural Earth Excavation-Piers	Cu. Yds.	700	700
Borrow	Cu. Yds.	13,600	
Granular Borrow	Cu. Yds.	930	
Gravel Borrow-In Place Measurement	Cu. Yds.	5,500	
Gravel Base Course-In Place Measure	Cu. Yds.	1,570	
* Bituminous Concrete Surface Course-Type "A"	Ton	154	154
Portland Cement Concrete-Abutments & Retaining Walls	Cu. Yds.	464	469
Portland Cement Concrete-Piers	Cu. Yds.	283	283
Portland Cement Concrete-Roadway Sidewalk Slab-On Steel Bridges	Cu. Yds.	385	385
Portland Cement	Bbls.	1,700	1,700
Structural Steel-Fabricated and Delivered	L. S.	Lump Sum	Lump Sum
Structural Steel-Erection	L. S.	Lump Sum	Lump Sum
Structural Steel-Field Painting	L. S.	Lump Sum	Lump Sum
Reinforcing Steel-Delivered	Lbs.	166,400	166,400
Reinforcing Steel-Placing	Lbs.	166,400	166,400
Steel H-Beam Piles, 42 lbs./ft.	Lin. Ft.	1,735	1,735
Bridge Rail	Lin. Ft.	706	706
* Membrane Waterproofing	Sq. Yd.	1,405	1,405
Epoxy Resin Surface Sealant	Sq. Yd.	233	233
Slope Paving	Sq. Yd.	1,070	
Granite Bridge Curb	Lin. Ft.	728	728
Loam Borrow	Cu. Yds.	600	
Seeding-Method No. 2	Unit	65	
Hay Mulch	Ton	3.5	
Asphalt Mulch Binder	Gallon	1,800	

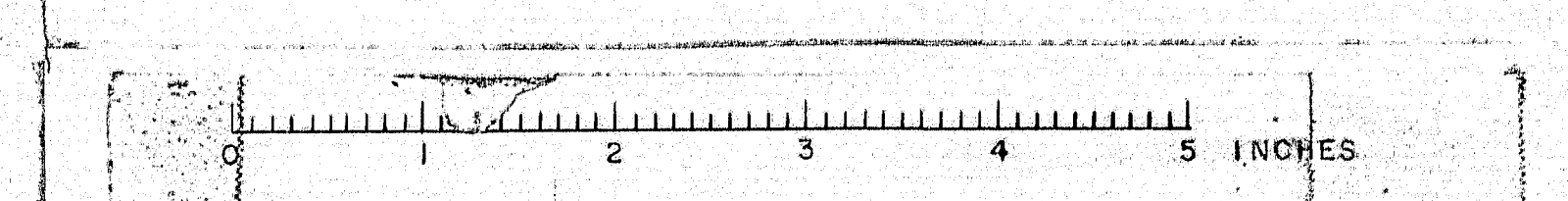
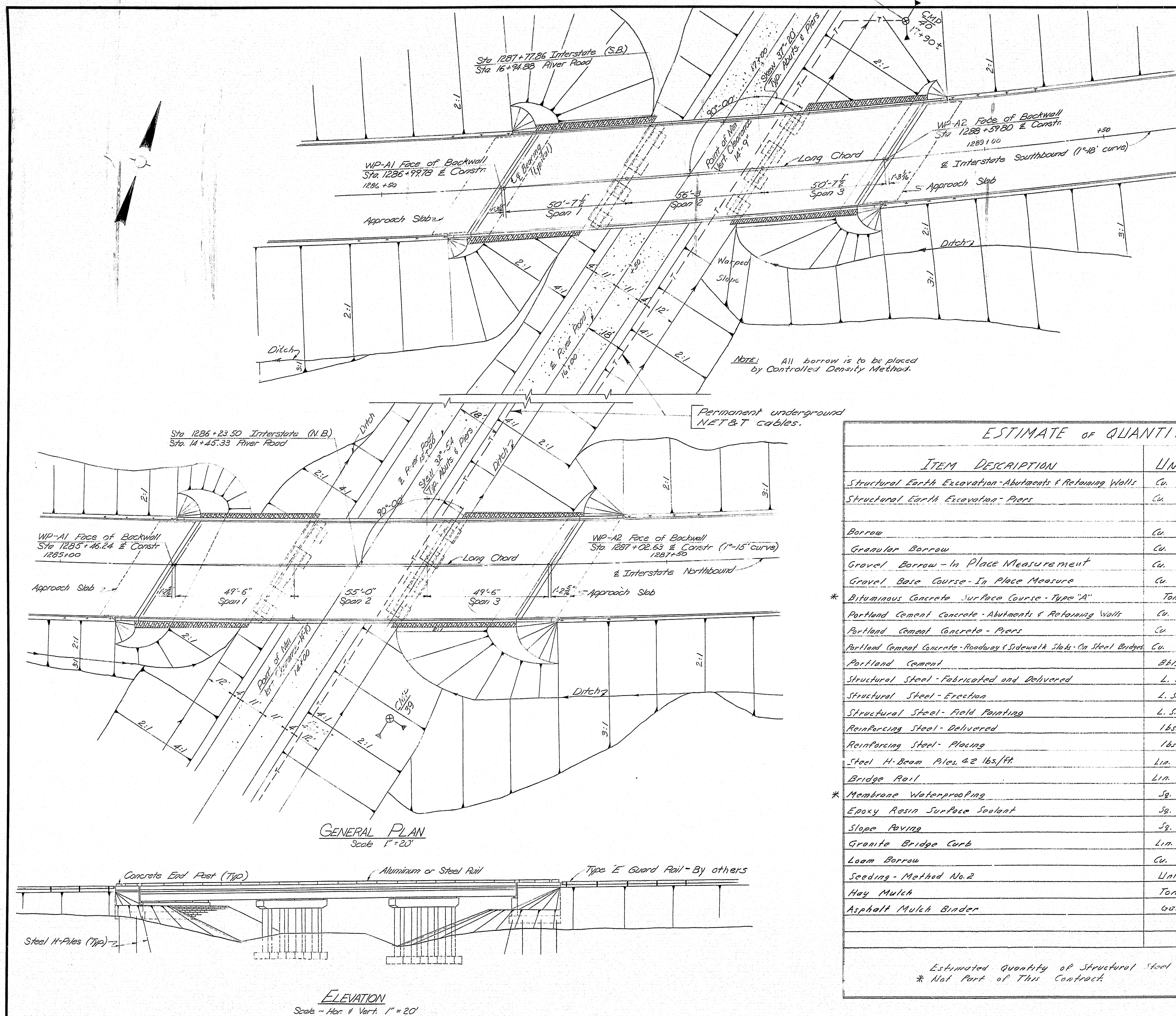
Estimated Quantity of Structural Steel = 296,600 lbs.
 * Not Part of This Contract

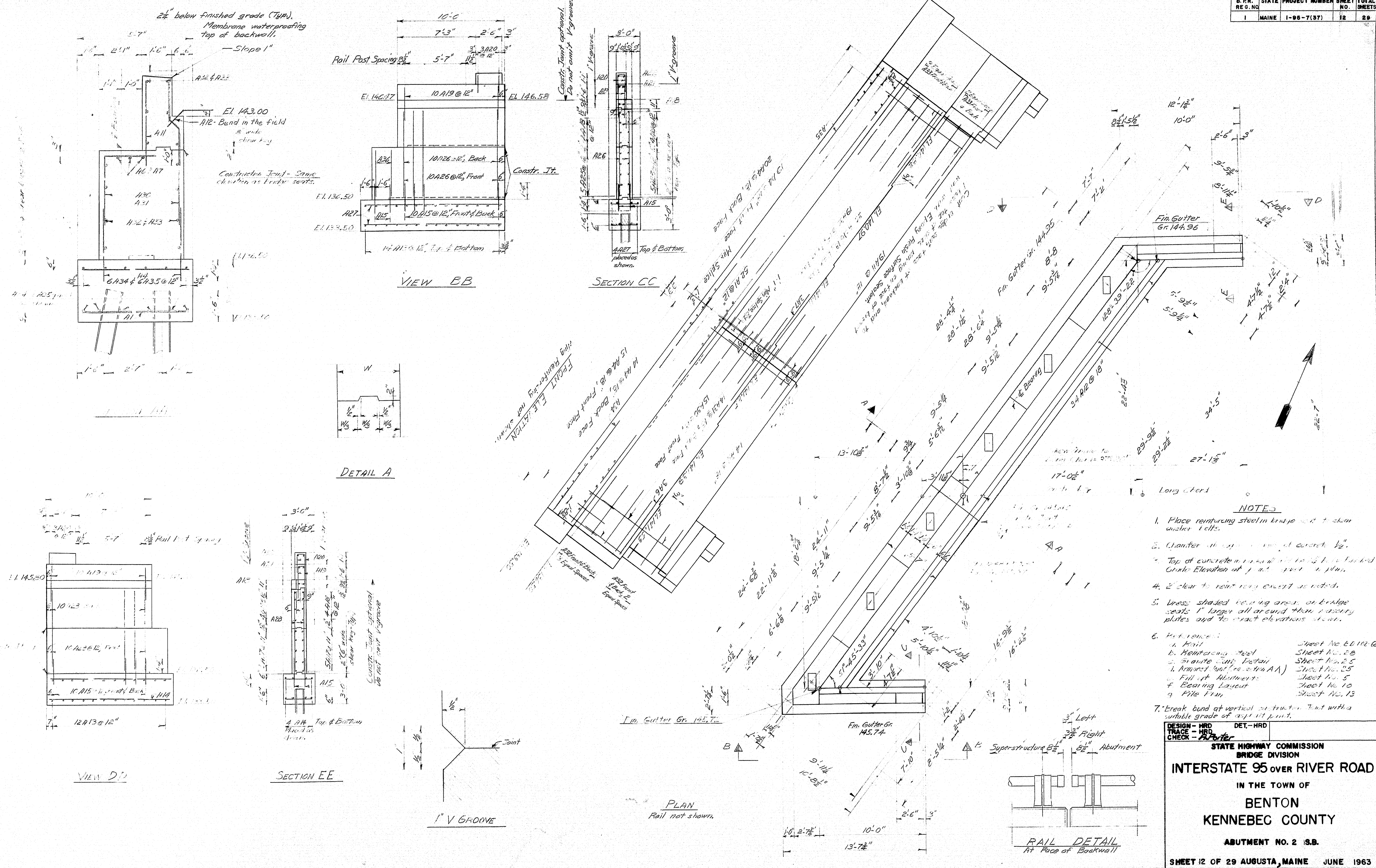
SPECIFICATIONS

DESIGN: A.A.S.H.O. Standard Specifications for Highway Bridges 1961, with interim revisions.
 CONSTRUCTION: State of Maine, State Highway Comm. Standard Specifications for Highways and Bridges, revision of January 1955, with supplements.
 LOADINGS: H20-S16-44, modified for Interstate Structural Steel-136'-20,000, A7-L-18,000 Reinforcing Steel - f_y = 80,000 psi. Concrete - (n=10), f_c = 1,800 psi.

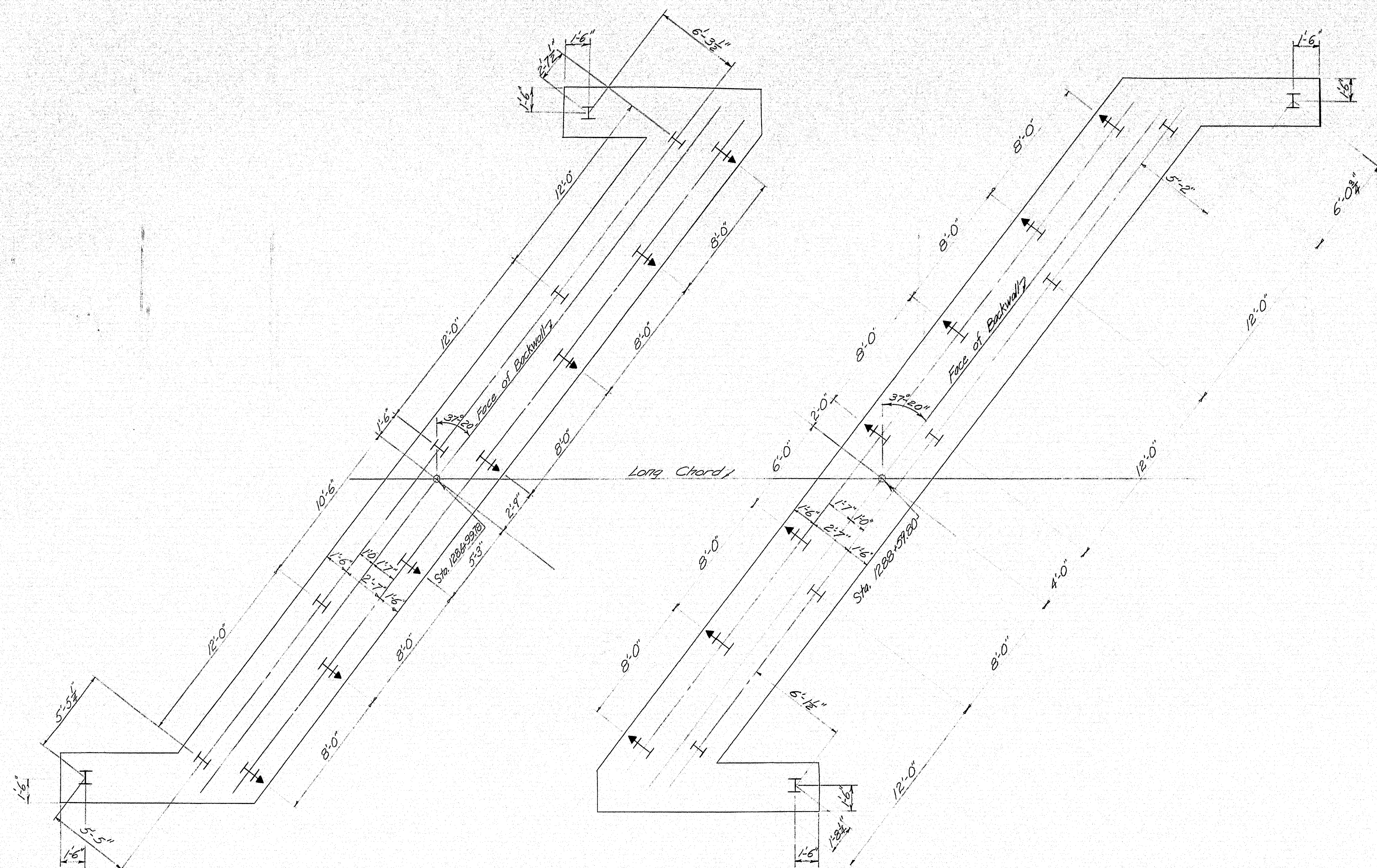
CONCRETE CLASSIFICATIONS: All concrete Class "A"

DETAIL-ROB TRACE-ROB CHECK-Drawn	BRIDGE NO. SURVEY- PLOT-
STATE HIGHWAY COMMISSION BRIDGE DIVISION INTERSTATE 95 OVER RIVER ROAD IN THE TOWN OF BENTON KENNEBEC COUNTY GENERAL PLAN QUANTITIES SHEET 10 OF 29 AUGUSTA, MAINE, JUNE 1963	





B. P. R. RES. NO.	STATE	PROJECT NUMBER	SHEET NO.	TOTAL SHEETS
1	MAINE	1-95-7(37)	13	29



PILE PLAN - ABUT. #1

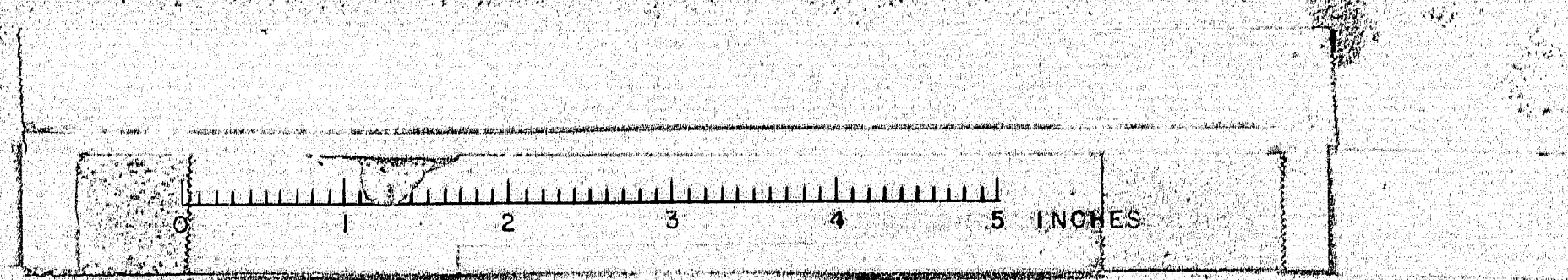
PILE PLAN - ABUT. #2

PILE NOTES

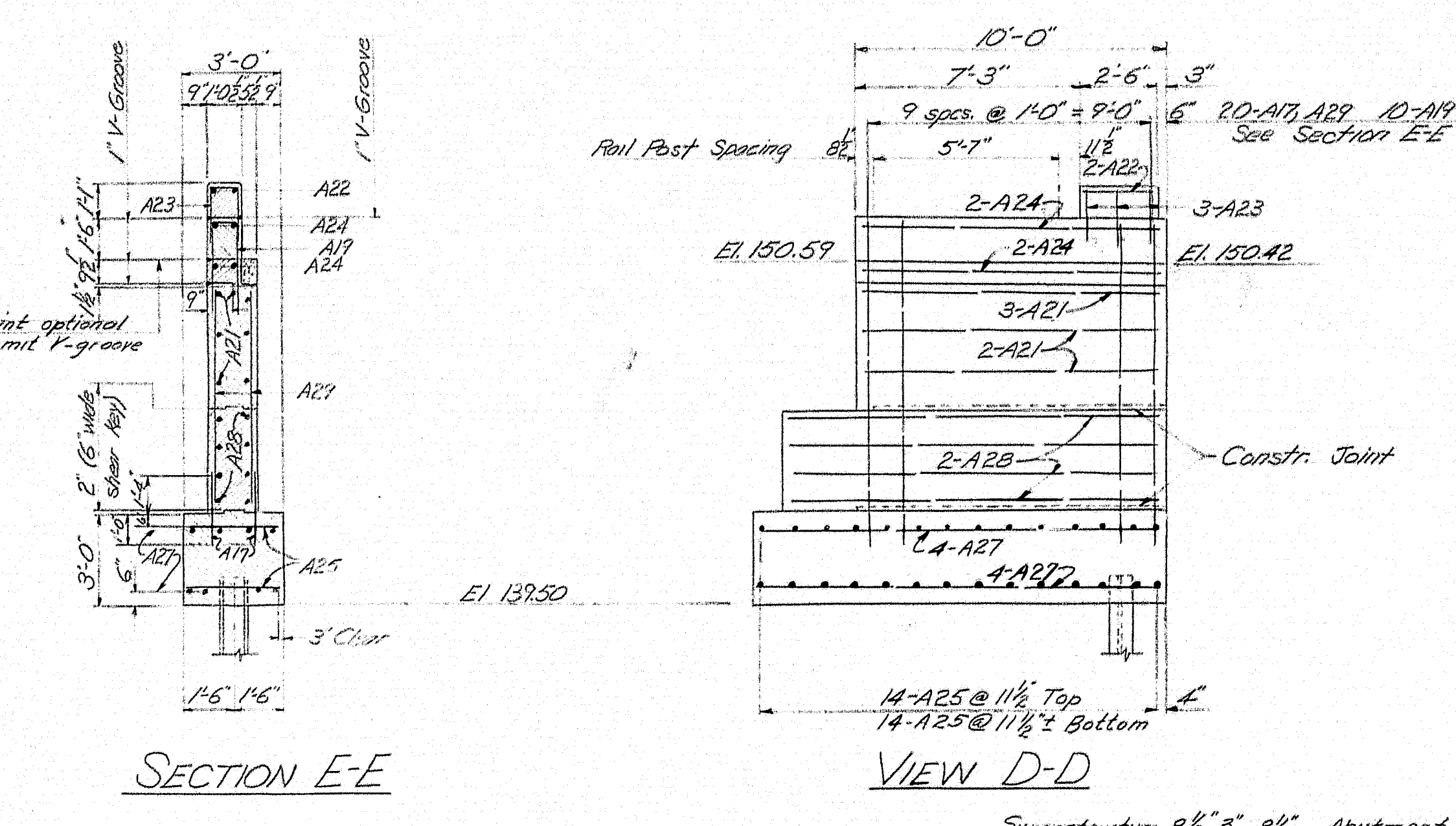
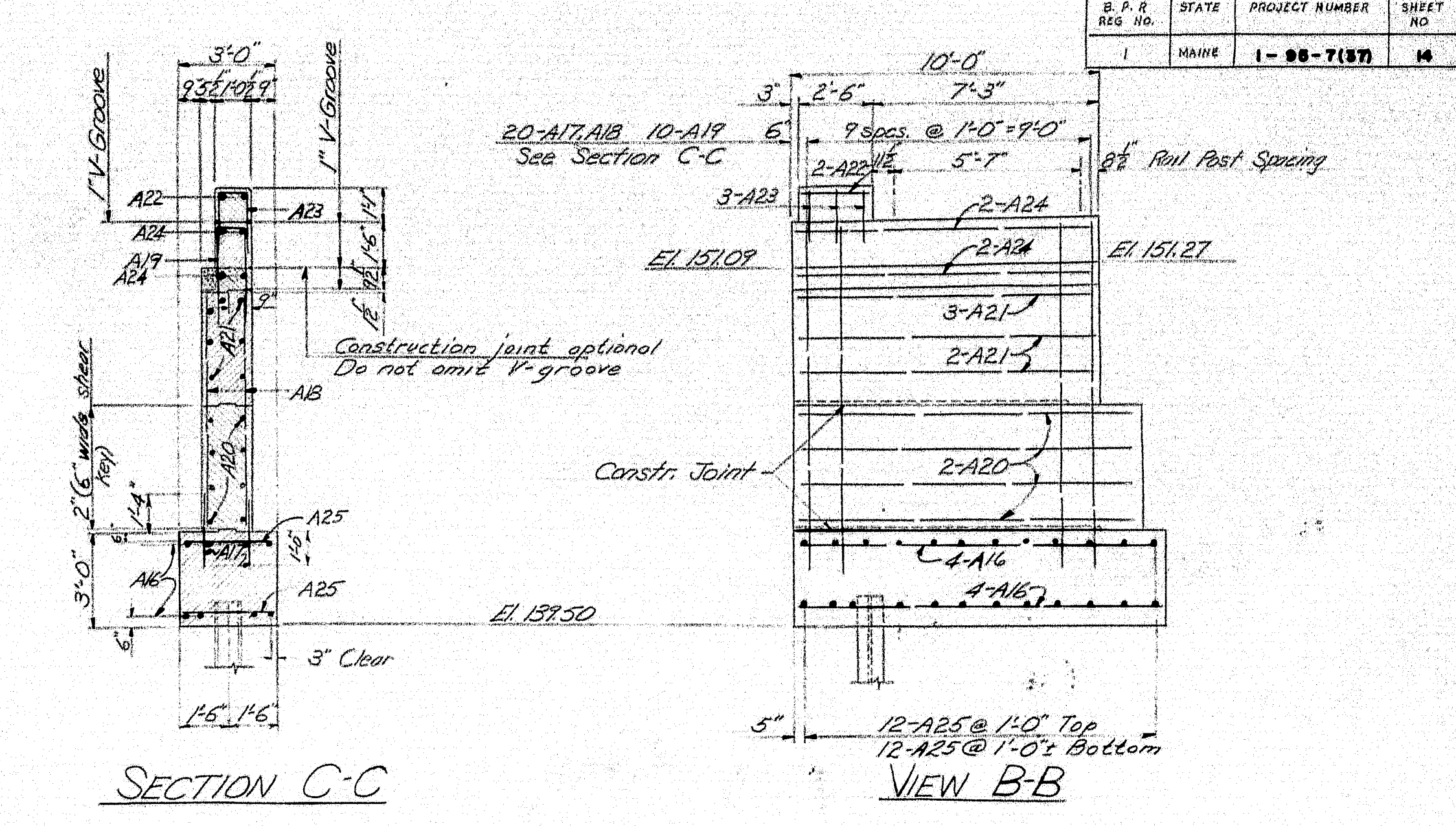
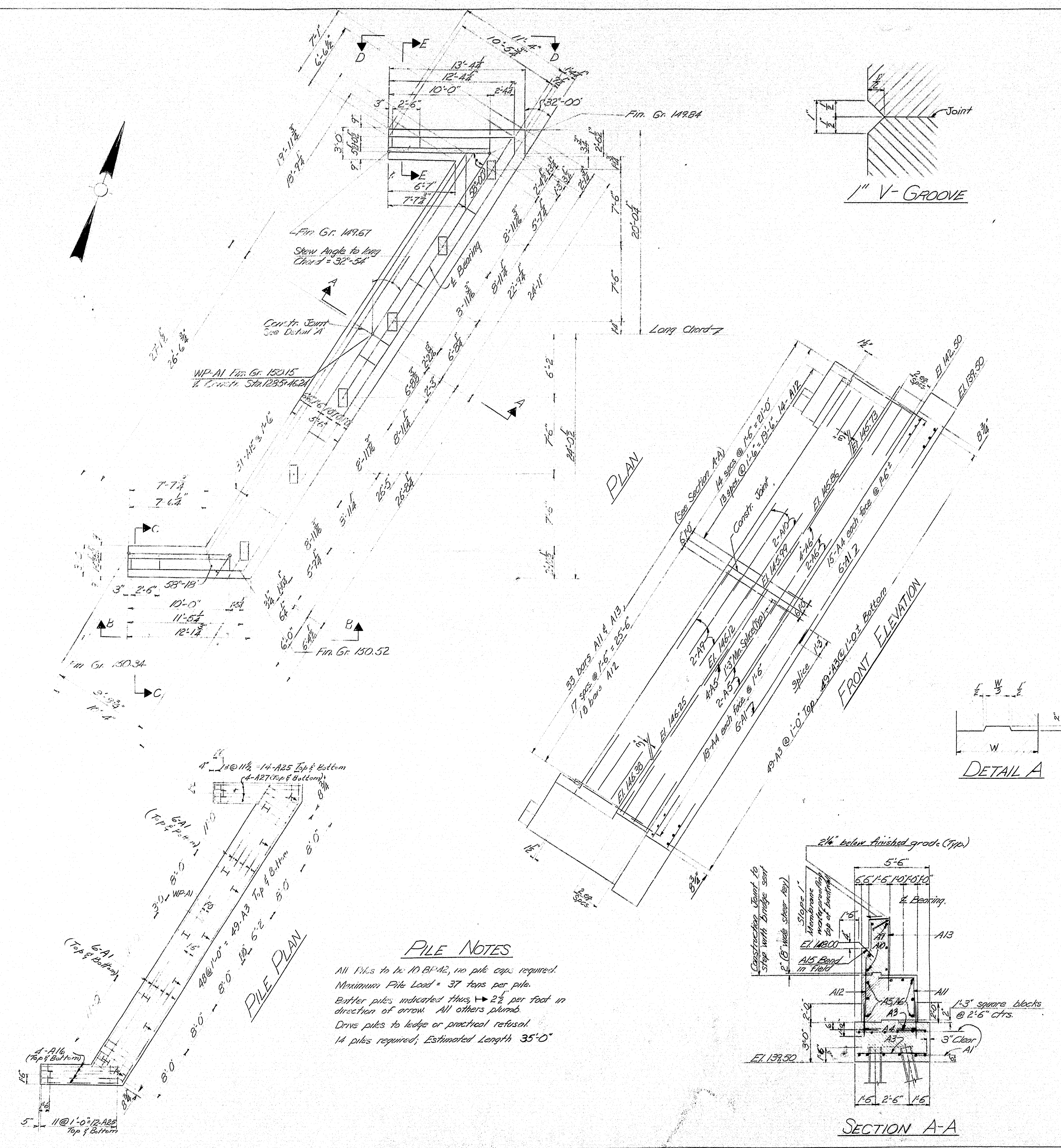
All piles to be 10 BP 42, no pile caps required.
 Maximum pile load = 37 tons per pile.
 Drive piles to ledge or practical refusal.
 Batter piles indicated thus, \rightarrow 2' per foot in
 direction of arrow. All others plumb.
 28 piles required, Estimated length Abut. #1 - 14 @ 30',
 Abut. #2 - 14 @ 25'

DESIGN - HRO TRACE - HRO CHECK - HRO	BRIDGE NO. SURVEY - ELOT -
STATE HIGHWAY COMMISSION BRIDGE DIVISION	
INTERSTATE 95 OVER RIVER ROAD	
IN THE TOWN OF	
BENTON	
KENNEBEC COUNTY	
ABUT. PILE PLANS S.B.	
SHEET 13 OF 29 AUGUSTA, MAINE JUNE 1963	

90-13



B. P. & R. REG. NO.	STATE	PROJECT NUMBER	SHEET NO.	TOTAL SHEETS
1	MAINE	1-95-7(17)	14	28

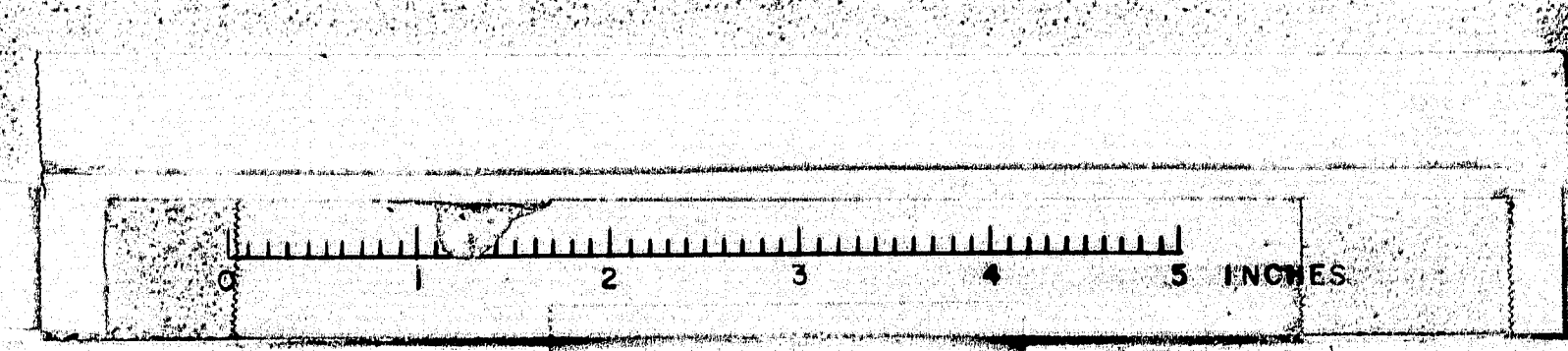


GENERAL NOTES

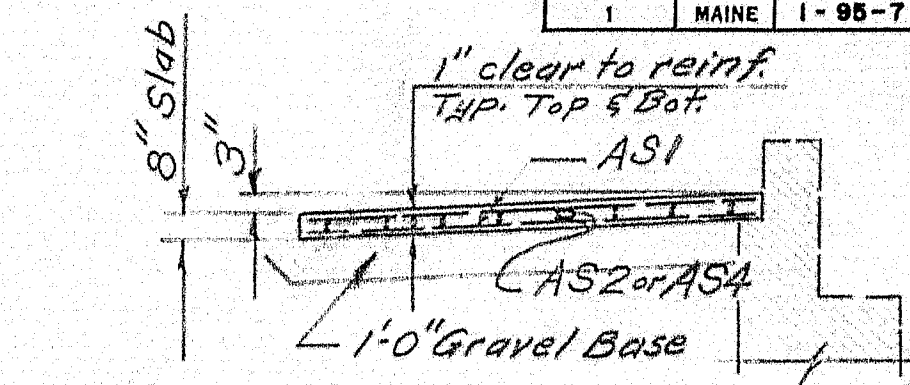
- Concrete in backwall 2' below finished grades at points shown.
- 2" clear to reinforcing except as noted.
- Dress shaded bearing areas on bridge seats, 1" lower all around than masonry plates and to exact elevations shown.
- Place reinforcing steel in bridge seat to clear anchor bolts.
- Coat bridge seat, face of backwall, 1' below top of slope paving on face of backwall with Epoxy Resin Surface Sealant.
- References:
 - a. Bridge Rail ———— Sh. No. BD-102-10
 - b. Reinforcing Steel ———— Sh. No. 29
 - c. Armored Joint (See Section A-A) ———— Sh. No. 27
 - d. Fill @ Abutments ———— Sh. No. 5
 - e. Granite Curb Detail ———— Sh. No. 27
- Break bond with a suitable grade of asphalt paint at the vertical construction joint.

RAIL DETAIL
At Face of Backwall

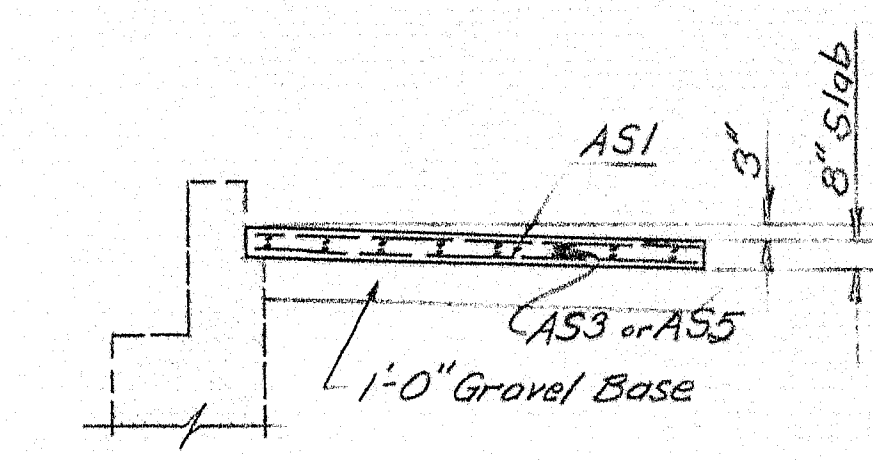
STATE HIGHWAY COMMISSION
BRIDGE DIVISION
INTERSTATE 95 OVER RIVER ROAD
IN THE TOWN OF
BENTON
KENNEBEC COUNTY
ABUTMENT NO. 1 N.B.
SHEET 14 OF 29 AUGUSTA, MAINE JUNE 1963
N.B. Abutment I-95 over River Road 90-14



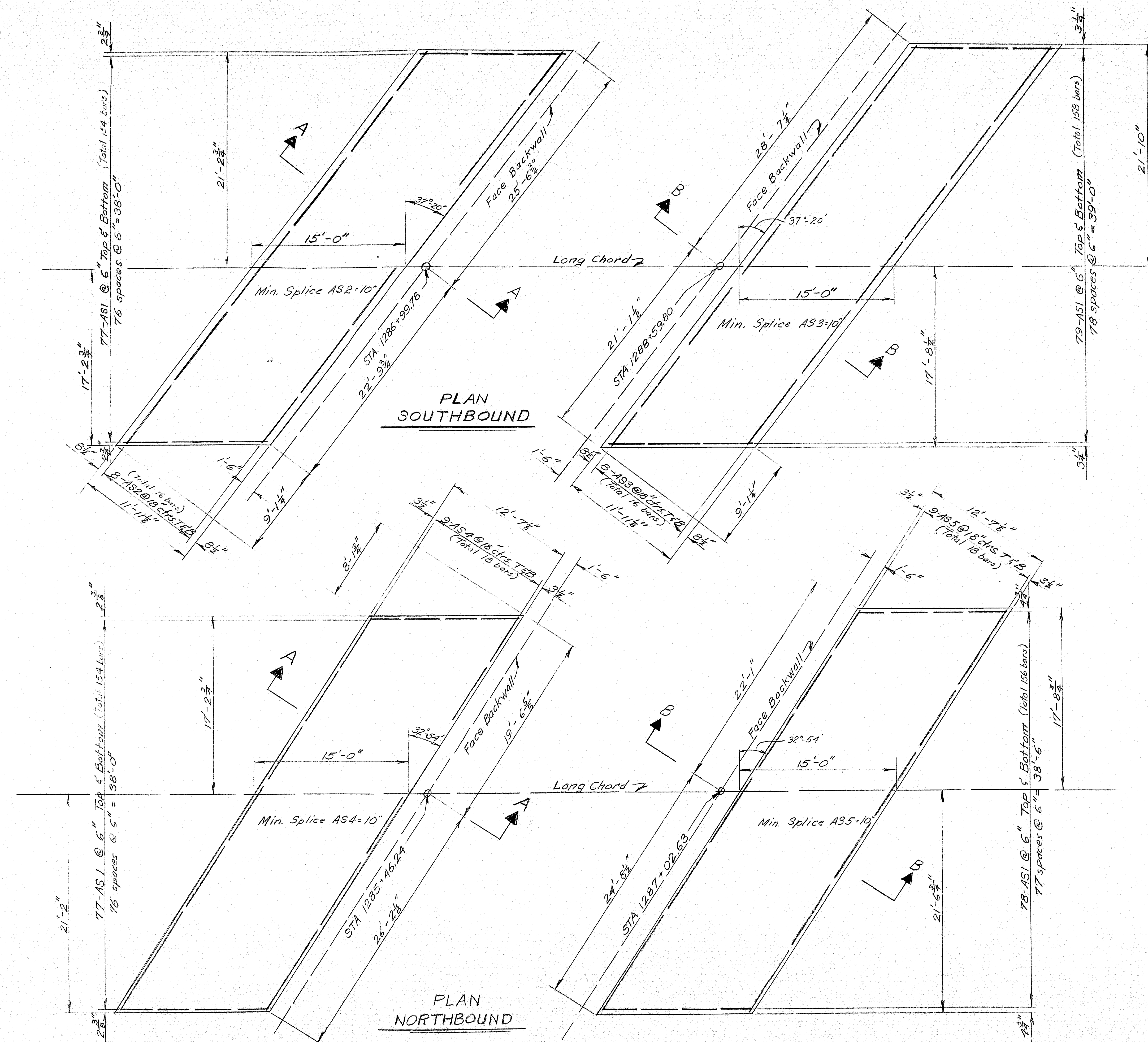
B.P. R. REV. NO.	STATE	PROJECT NUMBER	SHEET NO.	TOTAL SHEETS
1	MAINE	1-95-7(37)	16	29



SECTION A-A



SECTION B-B

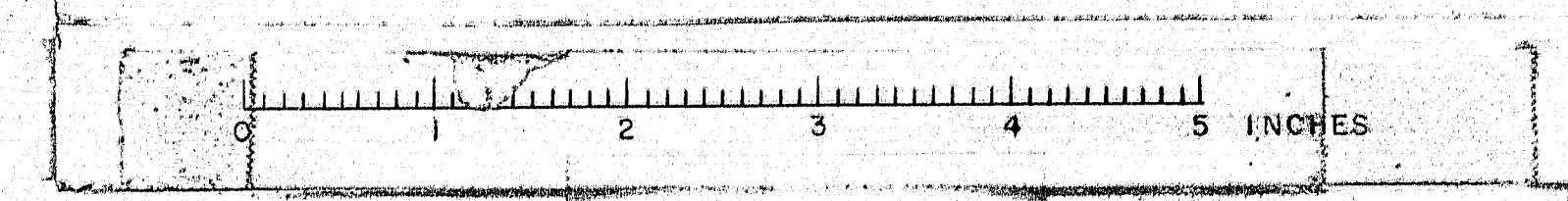


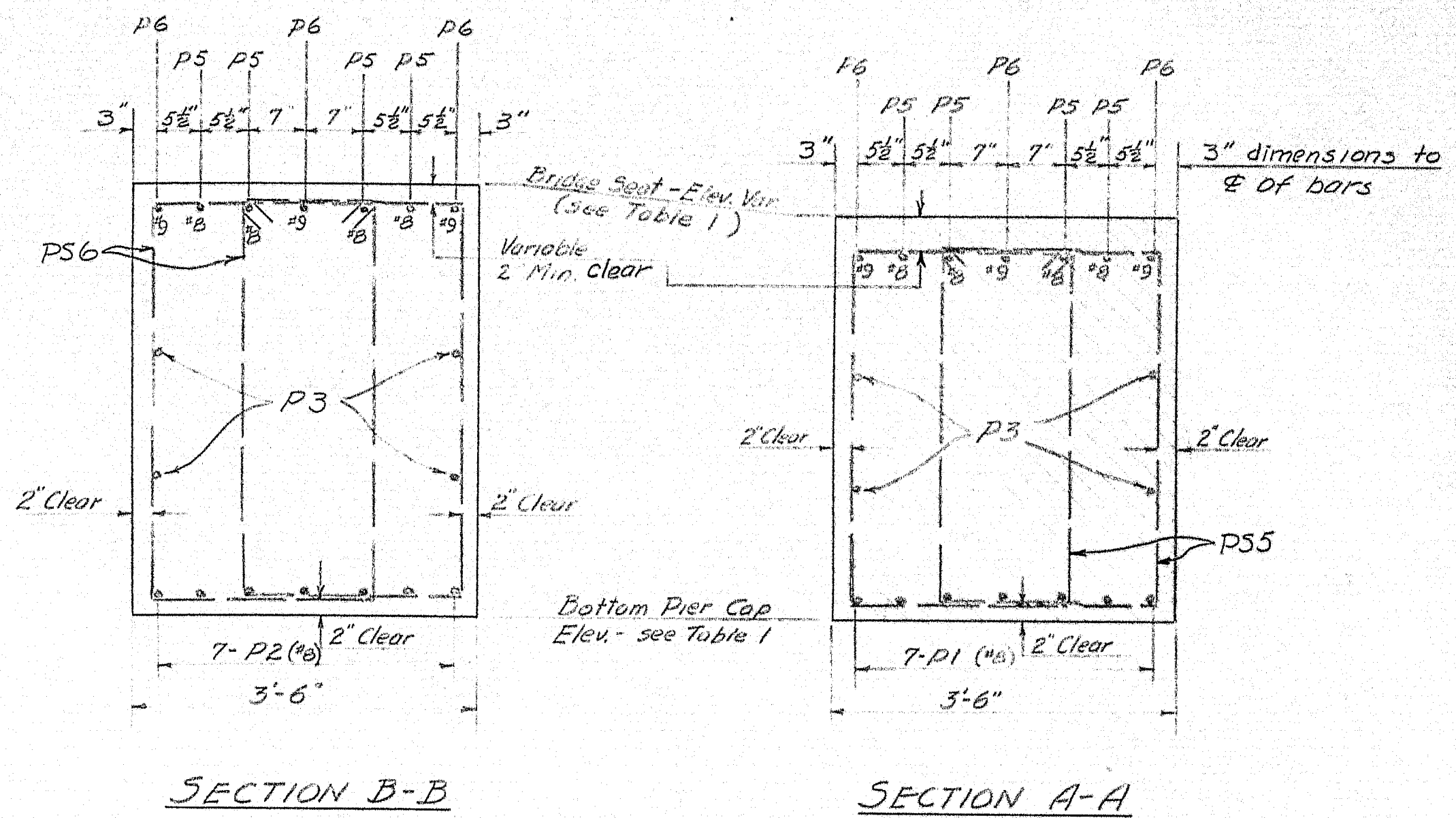
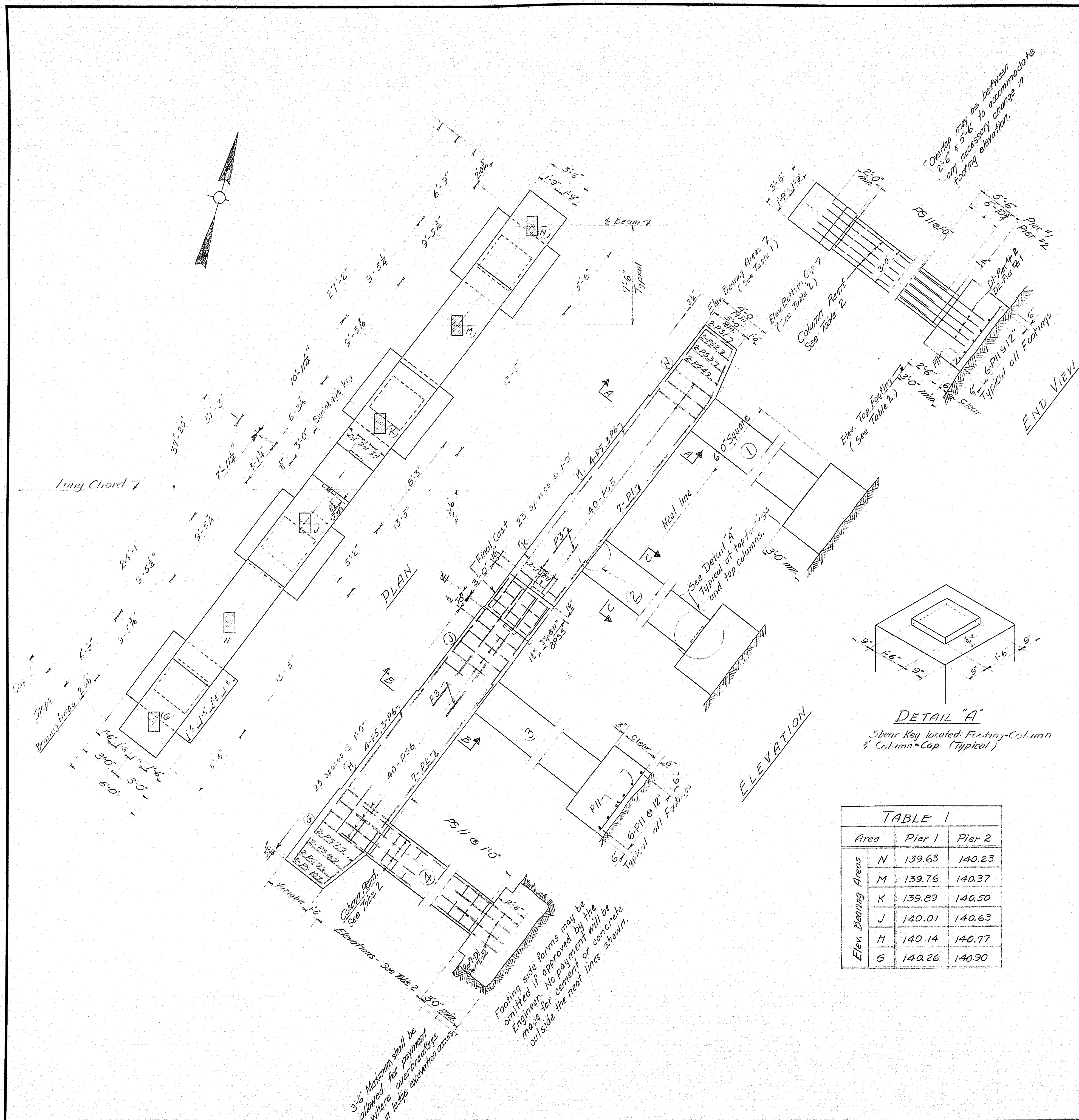
NOTE:
Payment for concrete in approach slabs to be made under Item 73-23 Portland Cement Concrete Retaining Walls and Retaining Walls.

DESIGN - AMR TRACE - RVL CHECK - A.B.P.	DET - RVL SURVEY - PLOT -	BRIDGE NO. STATE HIGHWAY COMMISSION BRIDGE DIVISION INTERSTATE 95 OVER RIVER ROAD IN THE TOWN OF BENTON KENNEBEC COUNTY APPROACH SLABS SB & NB
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SHEET 16 OF 29 AUGUSTA, MAINE JUNE 1963

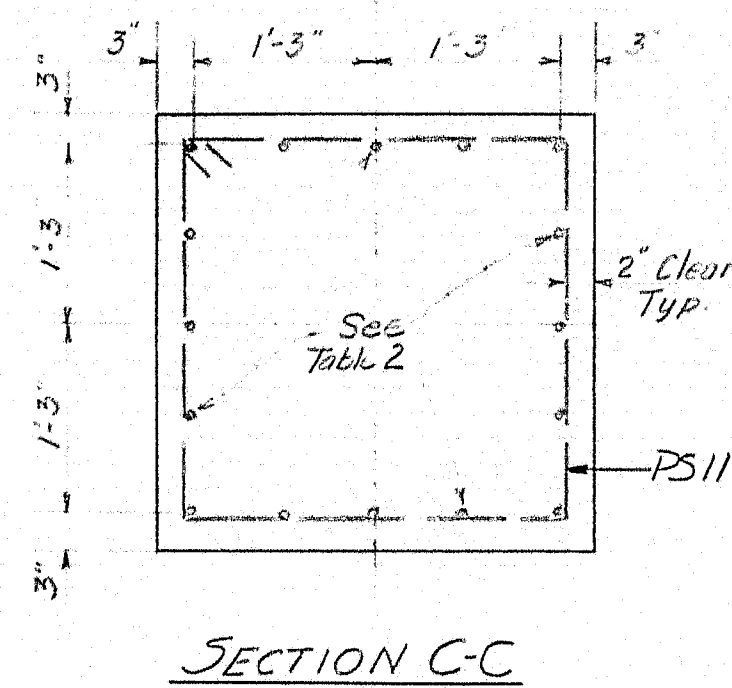
SB & NB APPROACH SLABS I-95 RIVER RD. 90-16



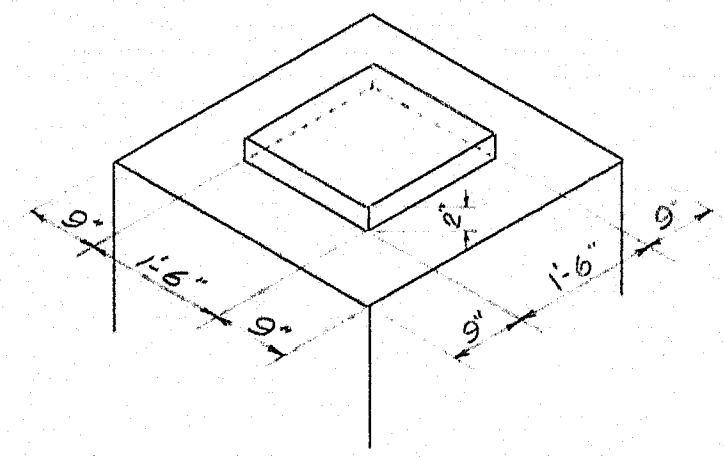


	Column Location	Column Reinf.	Column Stirrups	Top of Column El.	Top of Footing El.	Column Height	Approx Ledge El.
PIER 1	1	16-P22 ¹	15-P311	135.63	135.92	14'-7 1/2"	119.00
	2	16-P25 ¹	15-P311	135.63	136.98	15'-7 1/2"	117.00
	3	16-P24 ¹	18-P311	135.63	118.90	17'-7 1/2"	115.00
	4	16-P25 ¹	17-P311	135.63	117.90	16'-7 1/2"	115.00
PIER 2	1, 2, 3, 4	16-P26	18-P311	136.23	117.90	21'-2 1/2"	112.00
	2	16-P26	18-P311	136.23	119.00	17.23	
	3	16-P26	20-P311	136.23	117.00	18.23	
	4	16-P26	19-P311	136.23	117.50	18.73	

Note: For ledge elevations see Field Book 2, pages 49, 51.



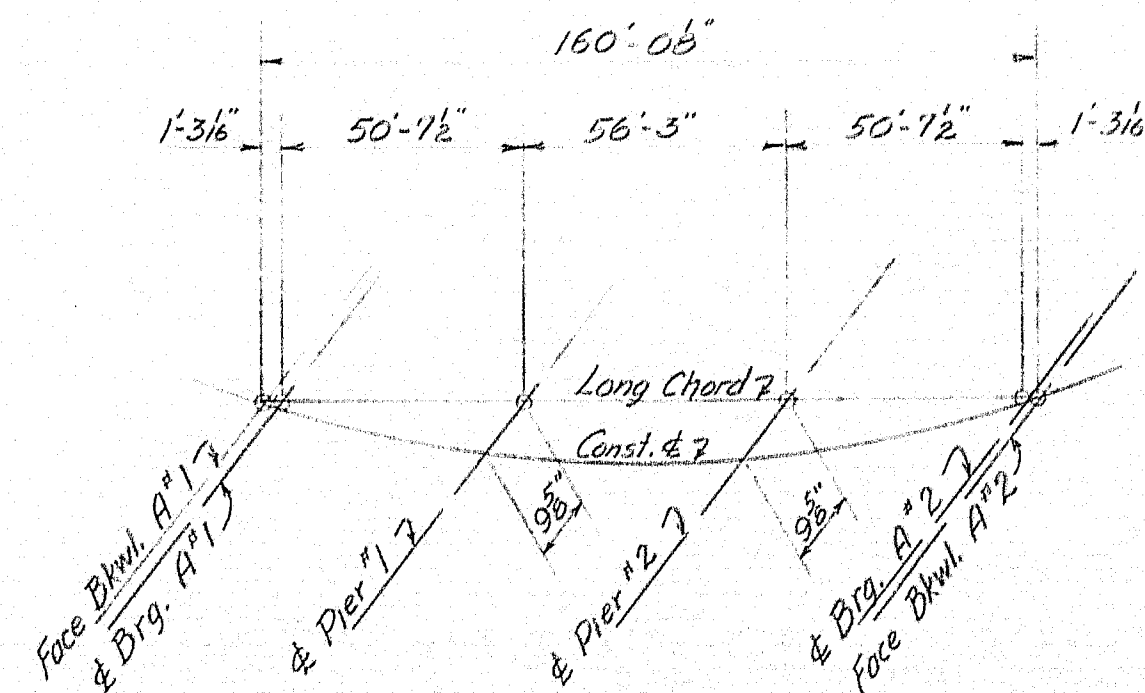
SECTION C-C



DETAIL "A"

Shear Key located: Fixing-Column
& Column-Cap (Typical)

Area	Pier 1	Pier 2
Elev. Bearing Areas	N 139.63	140.23
	M 139.76	140.37
	K 139.89	140.50
	J 140.01	140.63
	H 140.14	140.77
	G 140.26	140.90



LONG CHORD LAYOUT

Note: As built revisions shown in
black ink. 9-24-64 E. Barnard

NOTES

Dress shaded bearing areas to exact elevations shown and 1" larger all around than the required moscovy plate.

Reinforcing steel in top of cap to clear anchor bolts.

All weathered or broken ledge shall be removed before any footing concrete is placed.

Maximum footing Pressure = 14 Tons/sq. ft.
Top of footing elevations may be changed

Top of footing elevations may be changed by the Engineer in the field to fit ledge conditions.

The top of any footing shall not be less than 1'-0" below the surface of the final

Soils information indicates that some water

Soils information indicates that some water should be anticipated in pier foundation excavations. No direct payment will be made.

excavations. No direct payment will be made for unwatering foundations. This work should be considered as incidental to the contract.

be considered as incidental to the contract items.

DESIGN-HRD TRACE-FBF	DET.-FBF	BRIDGE NO. SURVEY-
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TRACE - FBI CHECK - T, H, K,	SURVEY PLOT -
STATE HIGHWAY COMMISSION	

STATE HIGHWAY COMMISSION
BRIDGE DIVISION
INTERSTATE OF RIVER ROAD

INTERSTATE 95 OVER RIVER ROAD

IN THE TOWN OF
BENTON

BENTON

KENNEBEC COUNTY

PIERS S.B.

SHEET 17 OF 29 AUGUSTA, MAINE JUNE 1963

PIERS S.B. 90-17

10-11

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1999, 2000, 2001, 2002, 2003, 2004, 2005, 2006, 2007, 2008, 2009, 2010, 2011, 2012, 2013, 2014, 2015, 2016, 2017, 2018, 2019, 2020, 2021, 2022, 2023, 2024, 2025, 2026, 2027, 2028, 2029, 2030, 2031, 2032, 2033, 2034, 2035, 2036, 2037, 2038, 2039, 2040, 2041, 2042, 2043, 2044, 2045, 2046, 2047, 2048, 2049, 2050, 2051, 2052, 2053, 2054, 2055, 2056, 2057, 2058, 2059, 2060, 2061, 2062, 2063, 2064, 2065, 2066, 2067, 2068, 2069, 2070, 2071, 2072, 2073, 2074, 2075, 2076, 2077, 2078, 2079, 2080, 2081, 2082, 2083, 2084, 2085, 2086, 2087, 2088, 2089, 2090, 2091, 2092, 2093, 2094, 2095, 2096, 2097, 2098, 2099, 2100, 2101, 2102, 2103, 2104, 2105, 2106, 2107, 2108, 2109, 2110, 2111, 2112, 2113, 2114, 2115, 2116, 2117, 2118, 2119, 2120, 2121, 2122, 2123, 2124, 2125, 2126, 2127, 2128, 2129, 2130, 2131, 2132, 2133, 2134, 2135, 2136, 2137, 2138, 2139, 2140, 2141, 2142, 2143, 2144, 2145, 2146, 2147, 2148, 2149, 2150, 2151, 2152, 2153, 2154, 2155, 2156, 2157, 2158, 2159, 2160, 2161, 2162, 2163, 2164, 2165, 2166, 2167, 2168, 2169, 2170, 2171, 2172, 2173, 2174, 2175, 2176, 2177, 2178, 2179, 2180, 2181, 2182, 2183, 2184, 2185, 2186, 2187, 2188, 2189, 2190, 2191, 2192, 2193, 2194, 2195, 2196, 2197, 2198, 2199, 2200, 2201, 2202, 2203, 2204, 2205, 2206, 2207, 2208, 2209, 2210, 2211, 2212, 2213, 2214, 2215, 2216, 2217, 2218, 2219, 2220, 2221, 2222, 2223, 2224, 2225, 2226, 2227, 2228, 2229, 2230, 2231, 2232, 2233, 2234, 2235, 2236, 2237, 2238, 2239, 2240, 2241, 2242, 2243, 2244, 2245, 2246, 2247, 2248, 2249, 2250, 2251, 2252, 2253, 2254, 2255, 2256, 2257, 2258, 2259, 2260, 2261, 2262, 2263, 2264, 2265, 2266, 2267, 2268, 2269, 2270, 2271, 2272, 2273, 2274, 2275, 2276, 2277, 2278, 2279, 2280, 2281, 2282, 2283, 2284, 2285, 2286, 2287, 2288, 2289, 2290, 2291, 2292, 2293, 2294, 2295, 2296, 2297, 2298, 2299, 2300, 2301, 2302, 2303, 2304, 2305, 2306, 2307, 2308, 2309, 2310, 2311, 2312, 2313, 2314, 2315, 2316, 2317, 2318, 2319, 2320, 2321, 2322, 2323, 2324, 2325, 2326, 2327, 2328, 2329, 2330, 2331, 2332, 2333, 2334, 2335, 2336, 2337, 2338, 2339, 2340, 2341, 2342, 2343, 2344, 2345, 2346, 2347, 2348, 2349, 2350, 2351, 2352, 2353, 2354, 2355, 2356, 2357, 2358, 2359, 2360, 2361, 2362, 2363, 2364, 2365, 2366, 2367, 2368, 2369, 2370, 2371, 2372, 2373, 2374, 2375, 2376, 2377, 2378, 2379, 2380, 2381, 2382, 2383, 2384, 2385, 2386, 2387, 2388, 2389, 2390, 2391, 2392, 2393, 2394, 2395, 2396, 2397, 2398, 2399, 2400, 2401, 2402, 2403, 2404, 2405, 2406, 2407, 2408, 2409, 2410, 2411, 2412, 2413, 2414, 2415, 2416, 2417, 2418, 2419, 2420, 2421, 2422, 2423, 2424, 2425, 2426, 2427, 2428, 2429, 2430, 2431, 2432, 2433, 2434, 2435, 2436, 2437, 2438, 2439, 2440, 2441, 2442, 2443, 2444, 2445, 2446, 2447, 2448, 2449, 2450, 2451, 2452, 2453, 2454, 2455, 2456, 2457, 2458, 2459, 2460, 2461, 2462, 2463, 2464, 2465, 2466, 2467, 2468, 2469, 2470, 2471, 2472, 2473, 2474, 2475, 2476, 2477, 2478, 2479, 2480, 2481, 2482, 2483, 2484, 2485, 2486, 2487, 2488, 2489, 2490, 2491, 2492, 2493, 2494, 2495, 2496, 2497, 2498, 2499, 2500, 2501, 2502, 2503, 2504, 2505, 2506, 2507, 2508, 2509, 2510, 2511, 2512, 2513, 2514, 2515, 2516, 2517, 2518, 2519, 2520, 2521, 2522, 2523, 2524, 2525, 2526, 2527, 2528, 2529, 2530, 2531, 2532, 2533, 2534, 2535, 2536, 2537, 2538, 2539, 2540, 2541, 2542, 2543, 2544, 2545, 2546, 2547, 2548, 2549, 2550, 2551, 2552, 2553, 2554, 2555, 2556, 2557, 2558, 2559, 2560, 2561, 2562, 2563, 2564, 2565, 2566, 2567, 2568, 2569, 2570, 2571, 2572, 2573, 2574, 2575, 2576, 2577, 2578, 2579, 2580, 2581, 2582, 2583, 2584, 2585, 2586, 2587, 2588, 2589, 2590, 2591, 2592, 2593, 2594, 2595, 2596, 2597, 2598, 2599, 2600, 2601, 2602, 2603, 2604, 2605, 2606, 2607, 2608, 2609, 2610, 2611, 2612, 2613, 2614, 2615, 2616, 2617, 2618, 2619, 2620, 2621, 2622, 2623, 2624, 2625, 2626, 2627, 2628, 2629, 2630, 2631, 2632, 2633, 2634, 2635, 2636, 2637, 2638, 2639, 2640, 2641, 2642, 2643, 2644, 2645, 2646, 2647, 2648, 2649, 2650, 2651, 2652, 2653, 2654, 2655, 2656, 2657, 2658, 2659, 2660, 2661, 2662, 2663, 2664, 2665, 2666, 2667, 2668, 2669, 2670, 2671, 2672, 2673, 2674, 2675, 2676, 2677, 2678, 2679, 2680, 26

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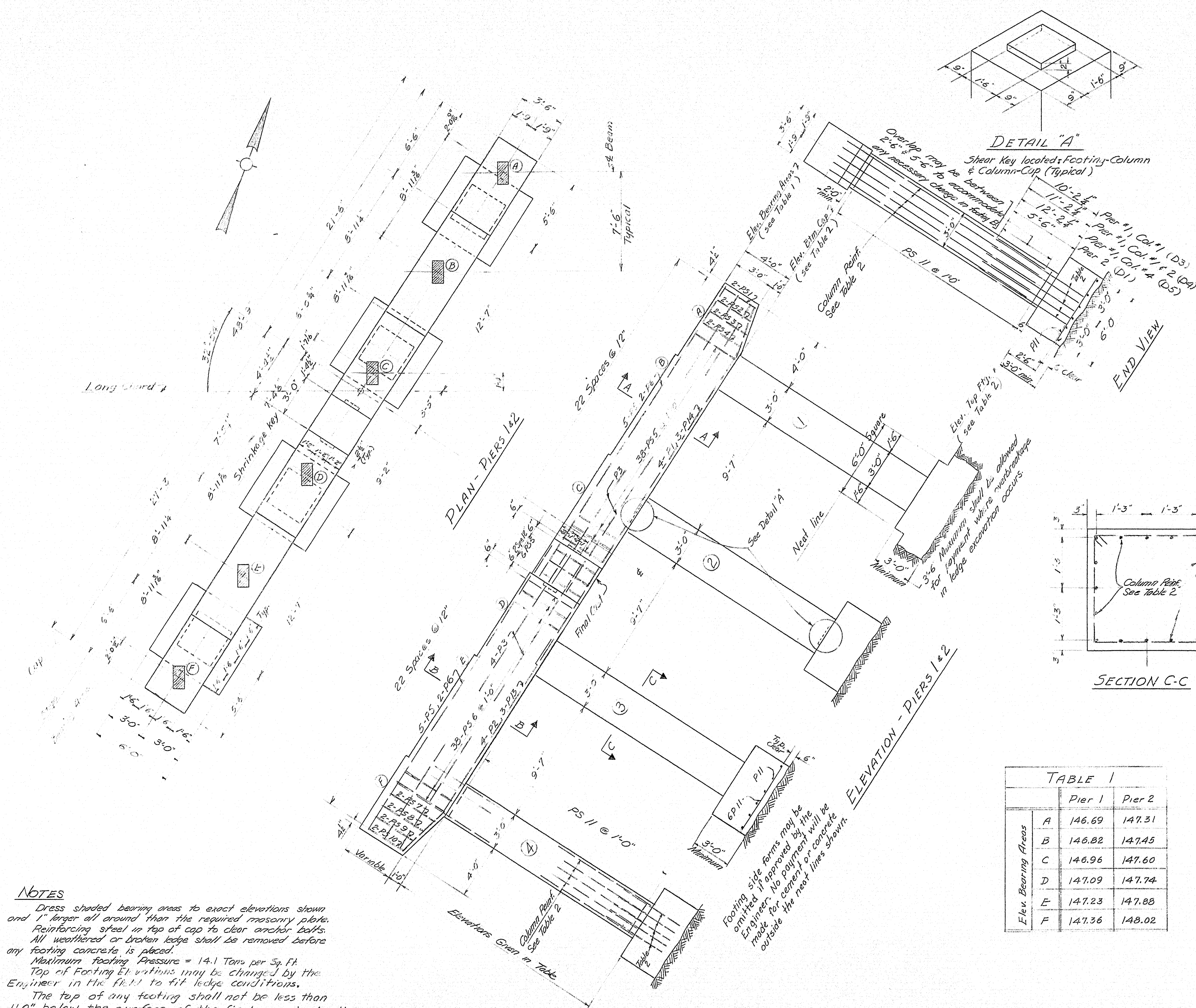


TABLE 2						
Column Location	Column Reinf.	Column Stirrups	Top of Footing Elev.	Top of Column Elev.	Column Height	Approx. Ledge Elev.
PIER 1	1	16-P15	25-PS11	142.69	24'-8"	115.00
	2	16-P15	26-PS11	142.69	25'-8"	114.00
	3	16-P15	26-PS11	142.69	25'-8"	114.00
	4	16-P15	27-PS11	142.69	26'-8"	113.00
PIER 2	1	16-P18	17-PS11	143.31	17'-3"	125.00
	2	16-P19	17-PS11	143.31	16'-9"	126.00
	3	16-P20	15-PS11	143.31	16'-3"	125.00
	4	16-P21	15-PS11	143.31	15'-3"	125.00

Note: For ledge elevations see Field Book 2, pages 48, 50.

NOTES

Dress shaded bearing areas to exact elevations shown and 1" larger all around than the required masonry plate. Reinforcing steel in top of cap to clear anchor bolts. All weathered or broken ledge shall be removed before any footing concrete is placed. Maximum footing pressure = 14.1 Tons per Sq. Ft. Top of footing elevations may be changed by the Engineer in the field to fit ledge conditions. The top of any footing shall not be less than 1'-0" below the surface of the final ground elevation.

TABLE 1		
	Pier 1	Pier 2
Elev. Bearing Areas		
A	146.69	147.31
B	146.82	147.45
C	146.96	147.60
D	147.09	147.74
E	147.23	147.88
F	147.36	148.02

DESIGN - M.R. DET-ADL, F.B.F.
TRACE - F.B.F.
CHECK - T.H.K.

BRIDGE NO. SURVEY - 101-
PILOT -

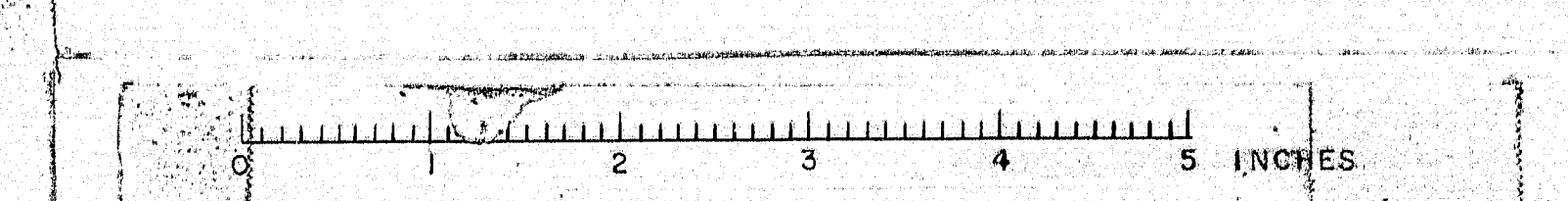
STATE HIGHWAY COMMISSION
BRIDGE DIVISION

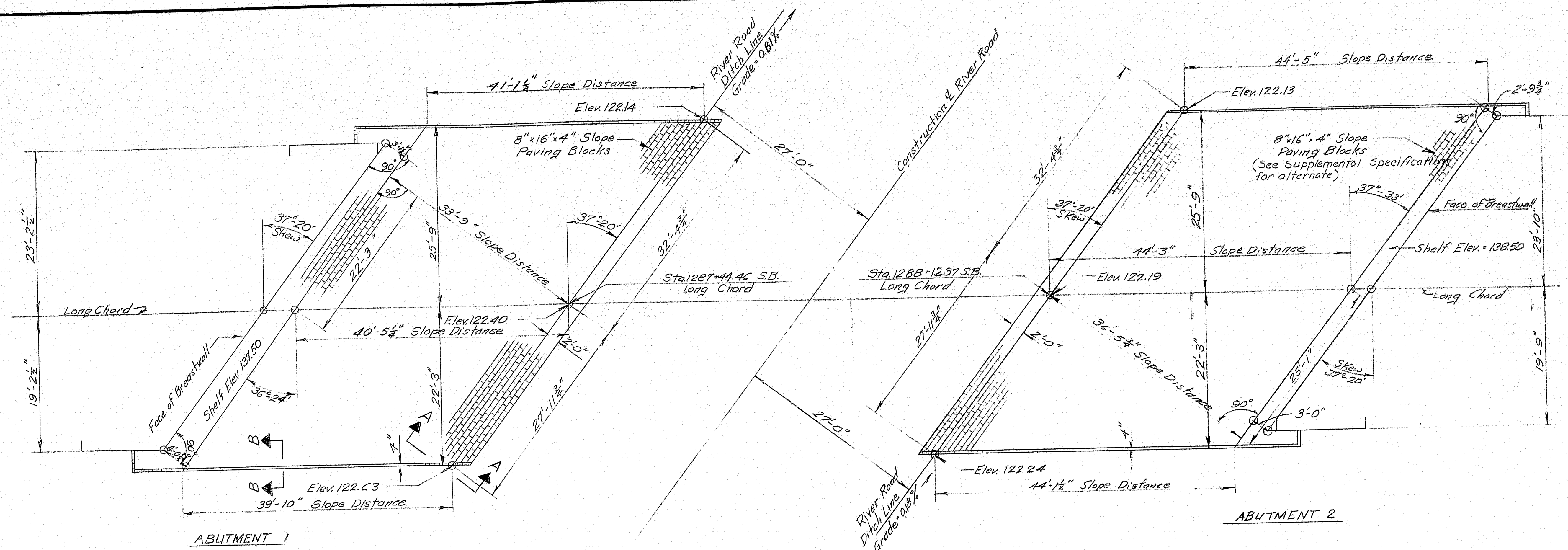
INTERSTATE 95 OVER RIVER ROAD
IN THE TOWN OF
BENTON
KENNEBEC COUNTY

PIERS N.B.

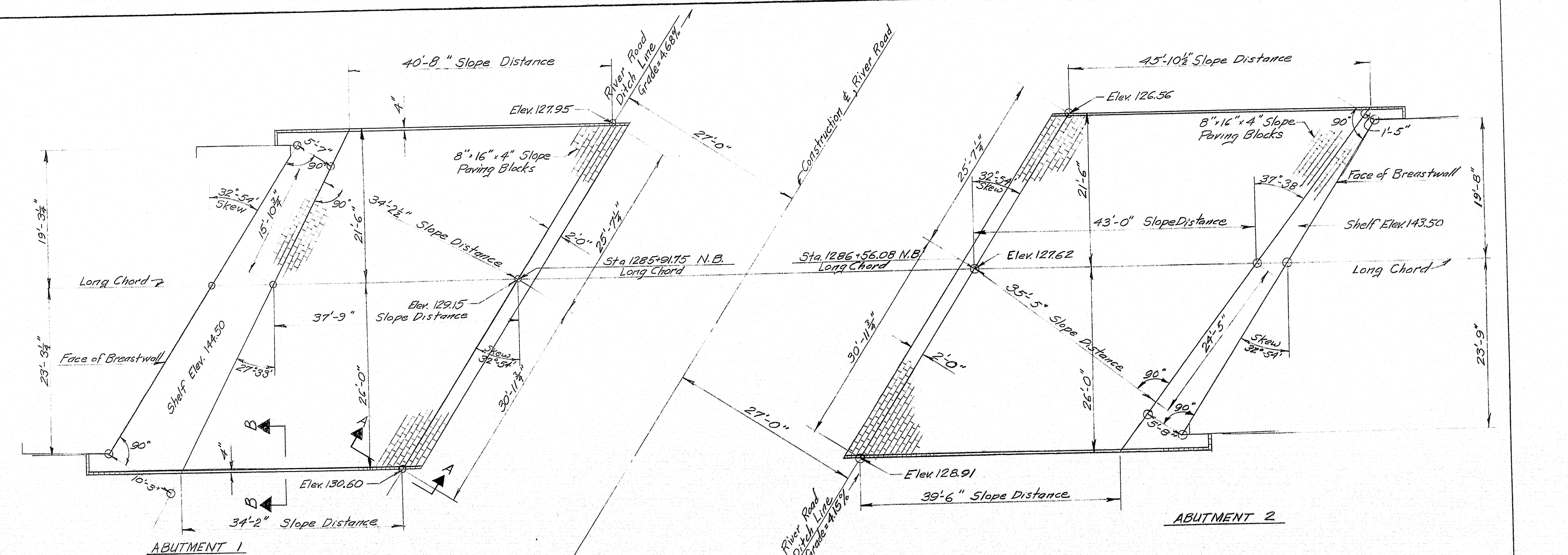
SHEET 18 OF 29 AUGUSTA, MAINE JUNE 1963

Note: As built revisions shown in black ink. 9-24-64 E. Barnard

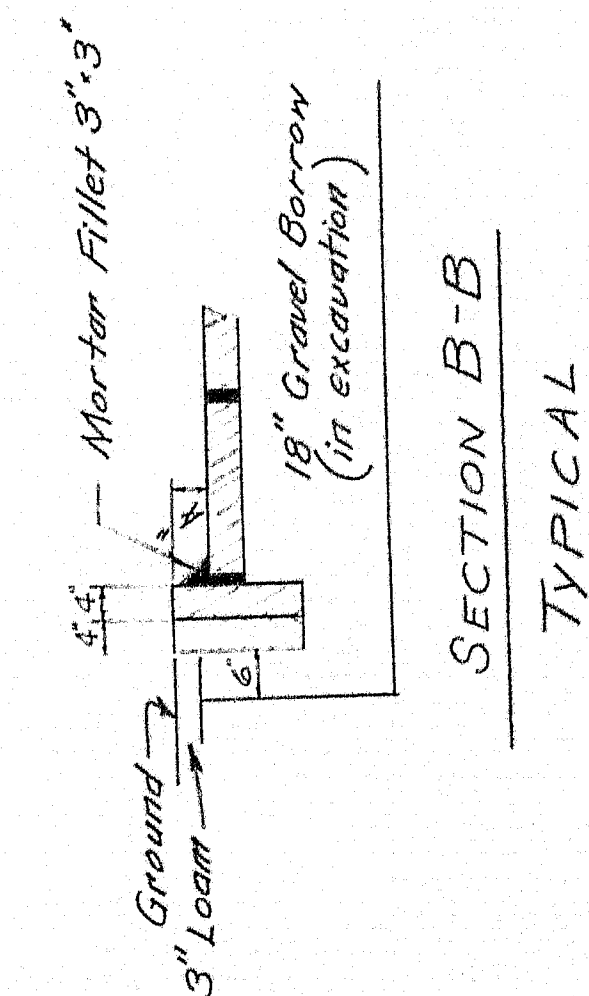
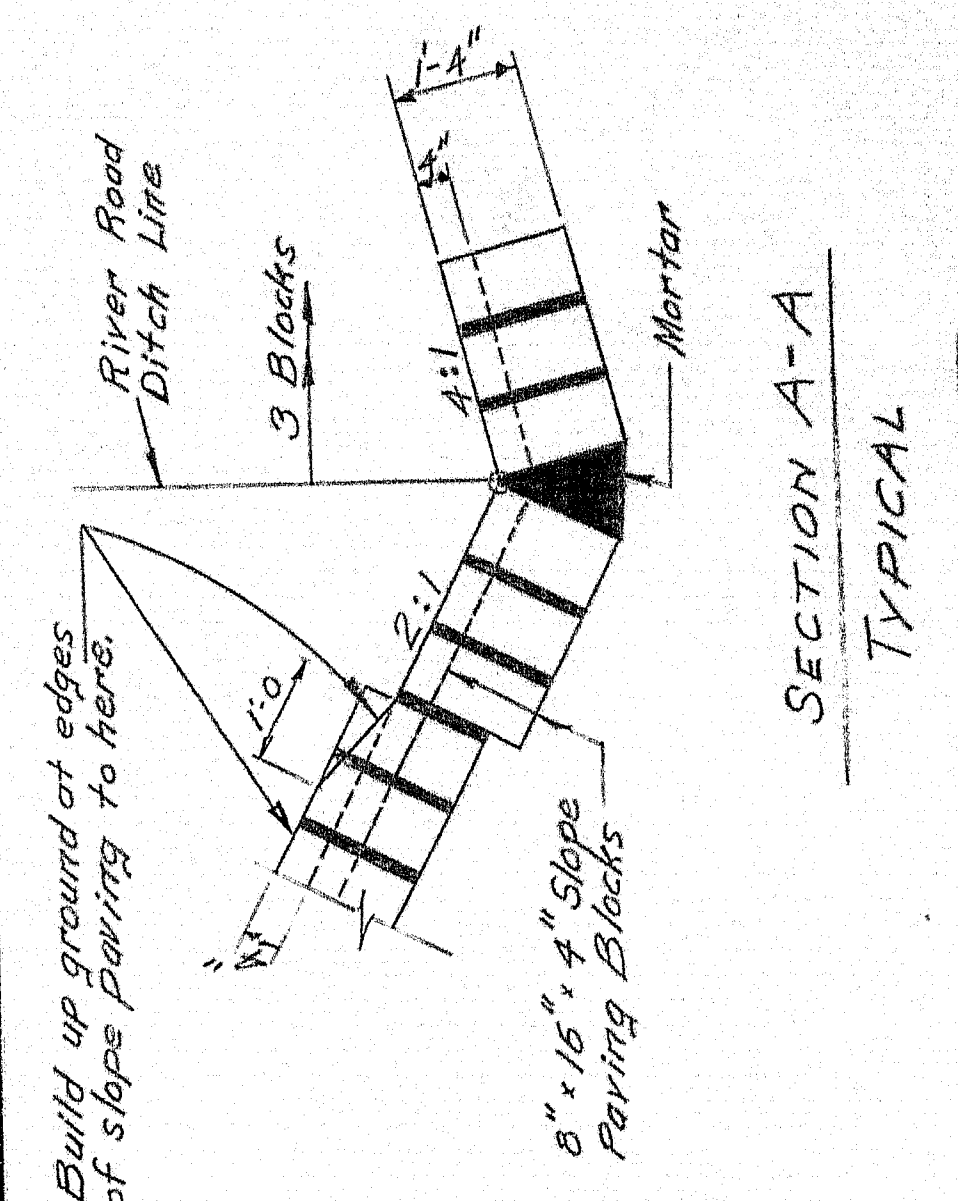




PLAN - SOUTHBOUND



PLAN - NORTHBOUND



NOTES:

- Excavation for 18" Gravel Boron (in excavated areas) under Slope paving to be paid for under Item 204-14 Structural Earth Excavation, Piers
- Depth of Gravel Boron under slope paving may be reduced or omitted in the opinion of the Engineer if the existing material is suitable.

DESIGN - A.H.R. H.R.D. DET. 20	BRIDGE NO. 19
TRACE - R.W.L.	SURVEY - 19
CHECK - D. B. B. B.	PLOT - 19

STATE HIGHWAY COMMISSION
BRIDGE DIVISION

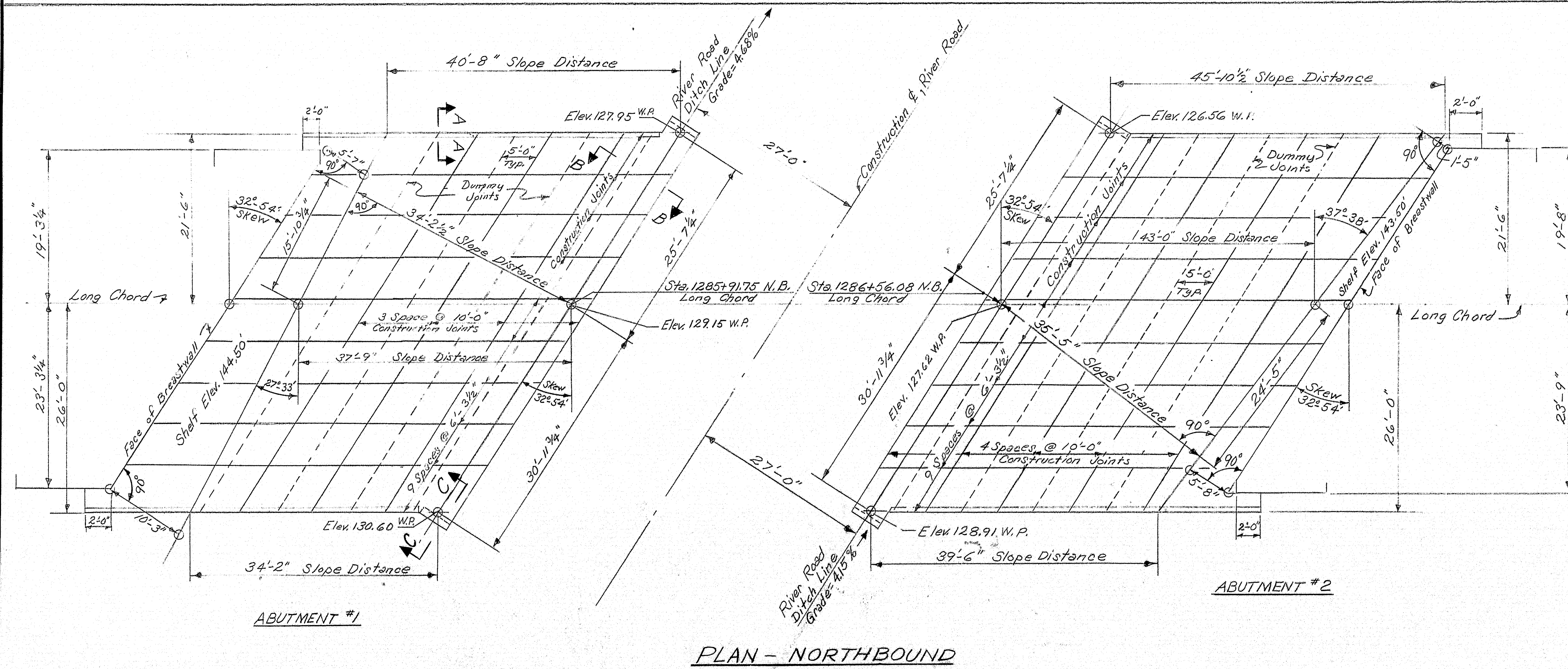
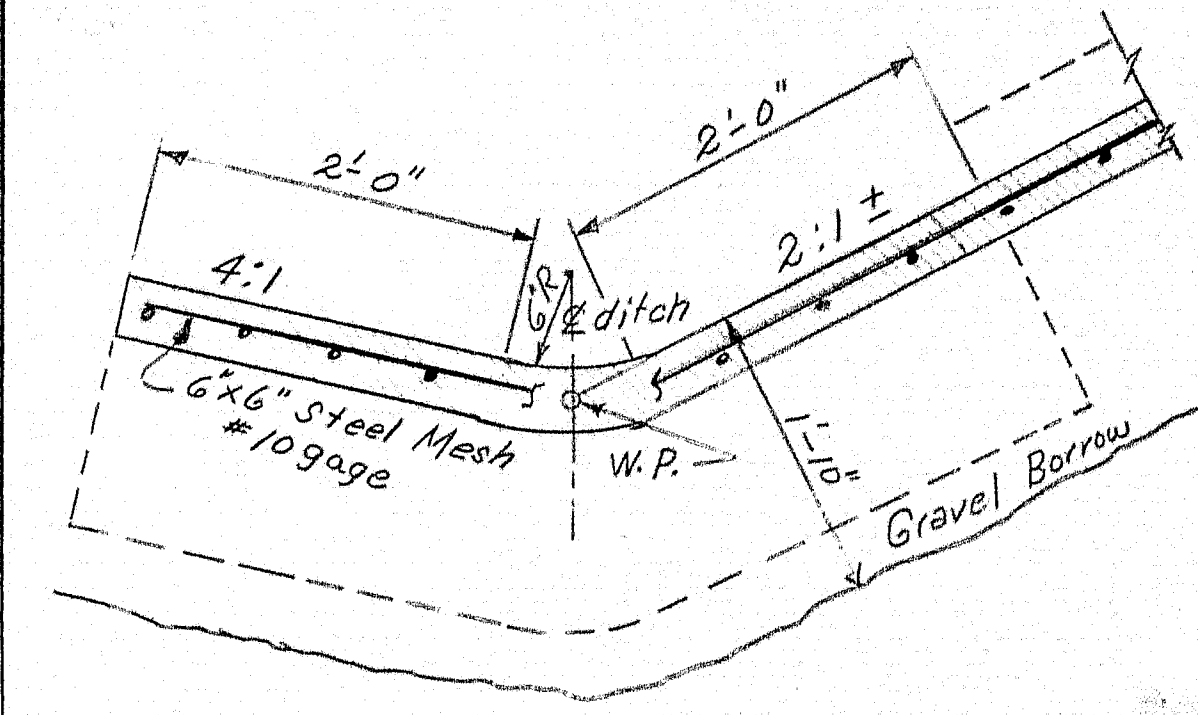
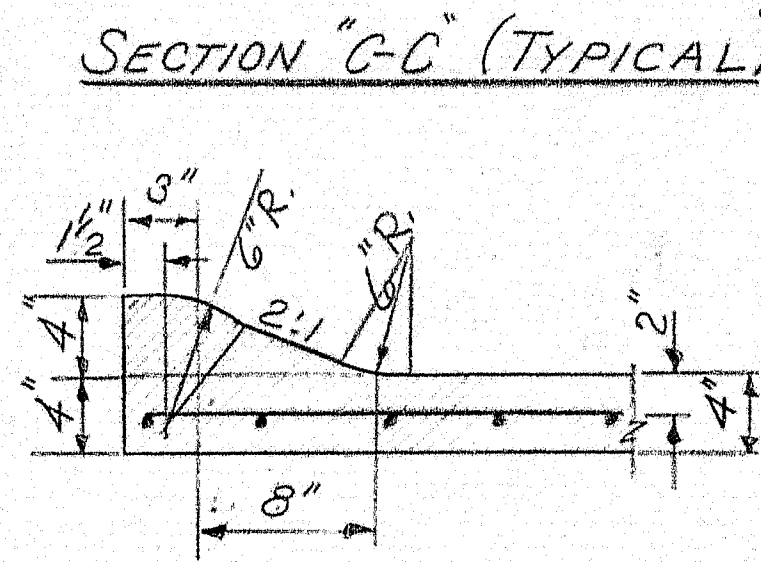
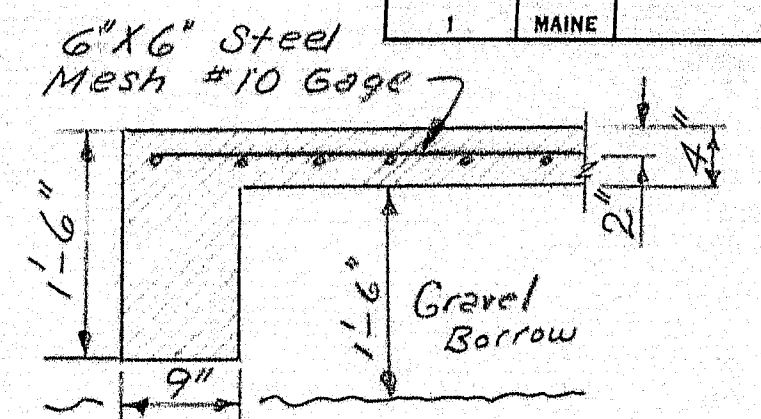
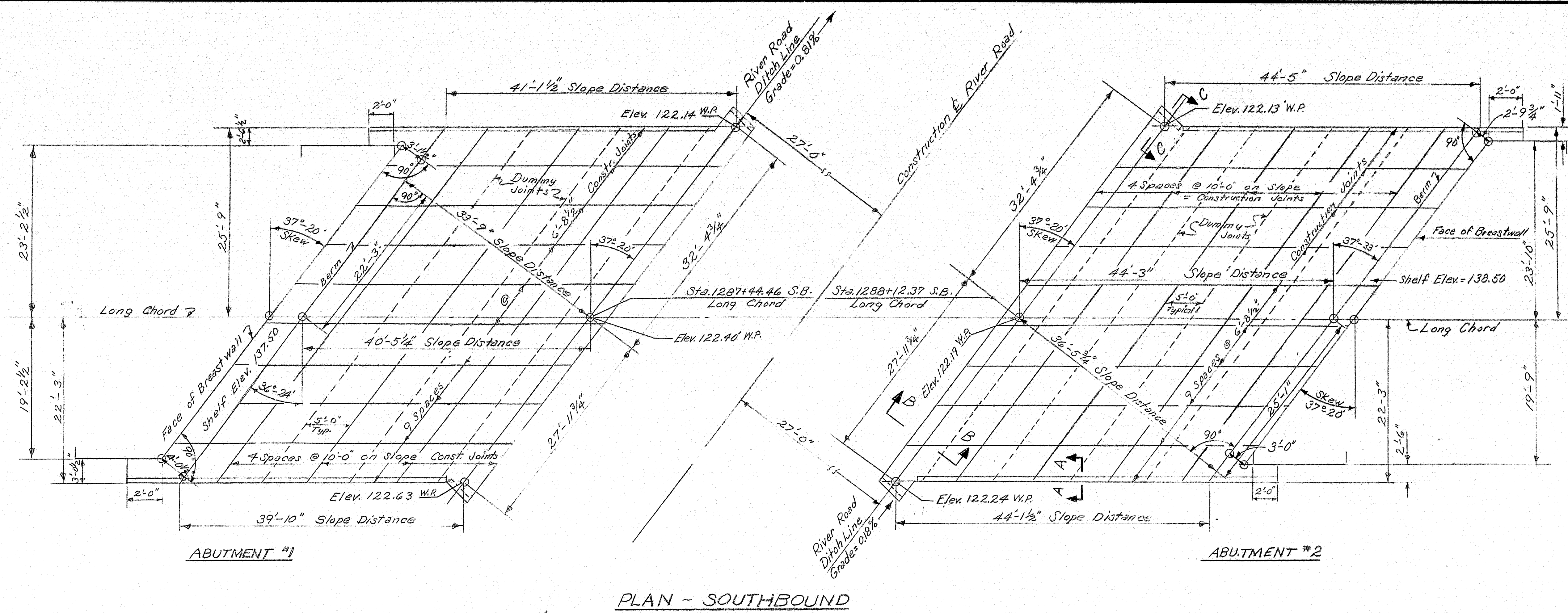
INTERSTATE 95 OVER RIVER ROAD

IN THE TOWN OF
BENTON
KENNEBEC COUNTY

SLOPE PAVING DETAILS SB & NB

SHEET 19 OF 29 AUGUSTA, MAINE JUNE 1963

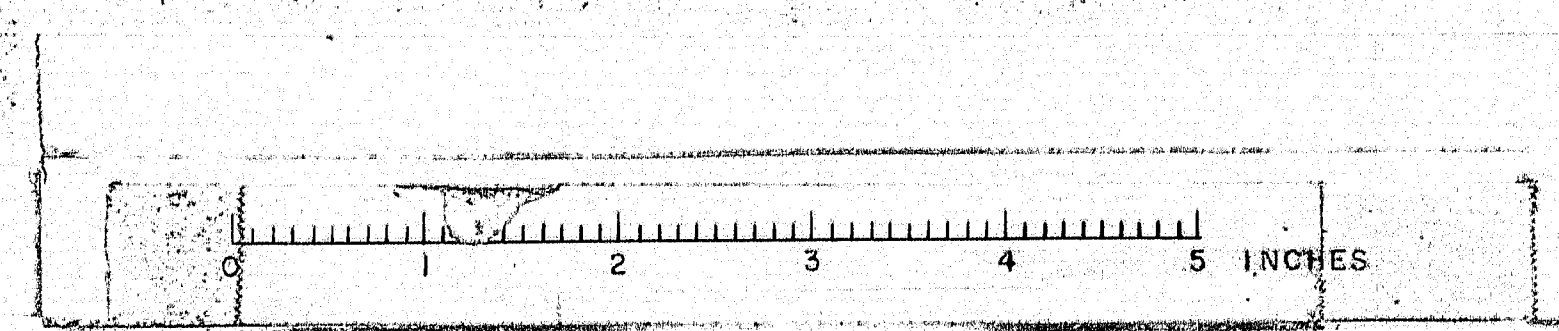
B. P. R. REG. NO.	STATE	PROJECT NUMBER	SHEET NO.	TOTAL SHEETS
1	MAINE			

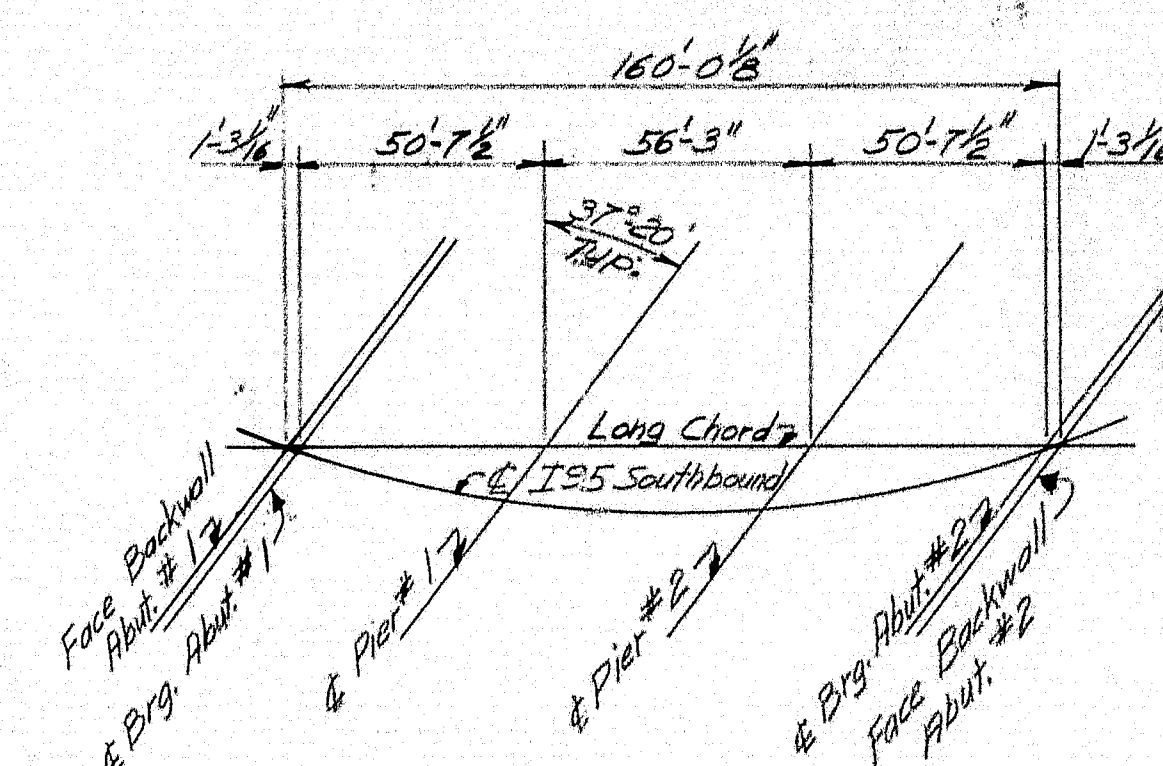
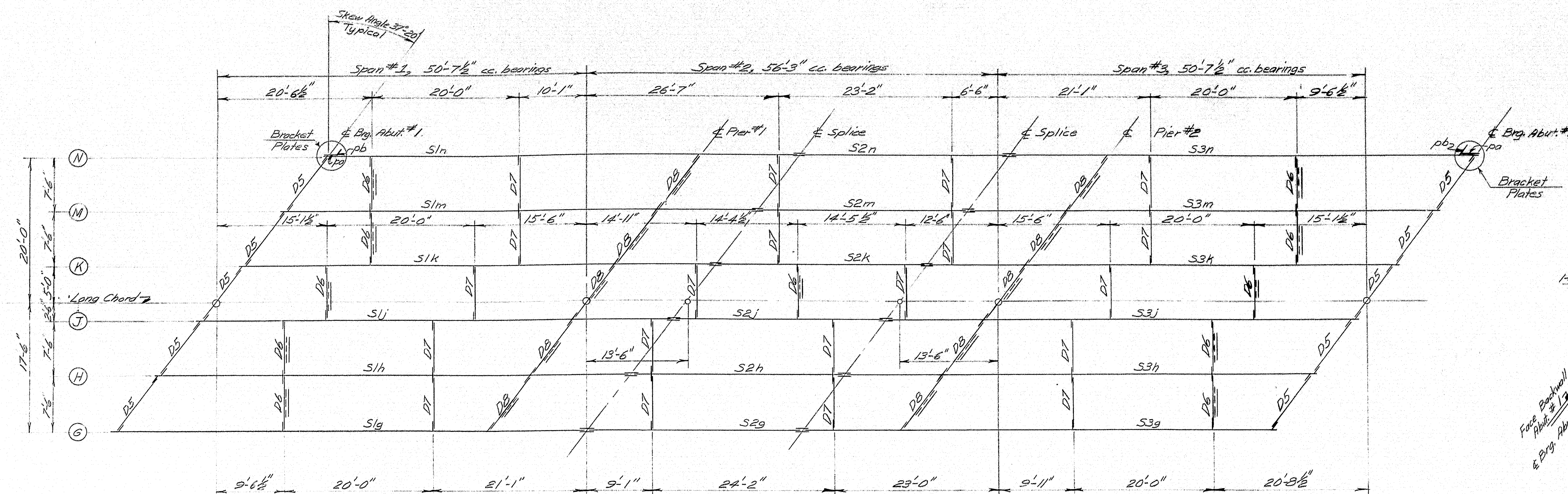


- NOTES:**
1. Break bond at Construction Joints with a coat of asphalt paint.
 2. Reinforce with #10 Gage 6"x6" Steel Mesh, not to pass through Construction Joints.
 3. At Contractors option, sections of this same strip may be cast in order. Bond will be broken between adjoining sections with 1/4" of Preformed Expansion Joint Filler.
 4. Dummy Joints shall be made with a side walk edging tool to a depth of 1/4".
 5. The 18" Gravel Borrow under this slope paving may be reduced or omitted, if in the opinion of the Engineer, the existing material is suitable.
 6. Payment for excavation for Gravel Borrow under Slope Paving to be made under Item 204-14, Structural Earth Excavation - Piers.

DESIGN - AHR, CWM	BRIDGE NO.
TRACE -	SURVEY -
CHECK - CDH	PLOT -
STATE HIGHWAY COMMISSION BRIDGE DIVISION	
INTERSTATE 95 OVER RIVER ROAD	
IN THE TOWN OF	
BENTON	
KENNEBEC COUNTY	
SLOPE PAVING DETAILS S.B. & N.B.	
SHEET 19A OF 29 AUGUSTA, MAINE NOV. 1963	

90-19A



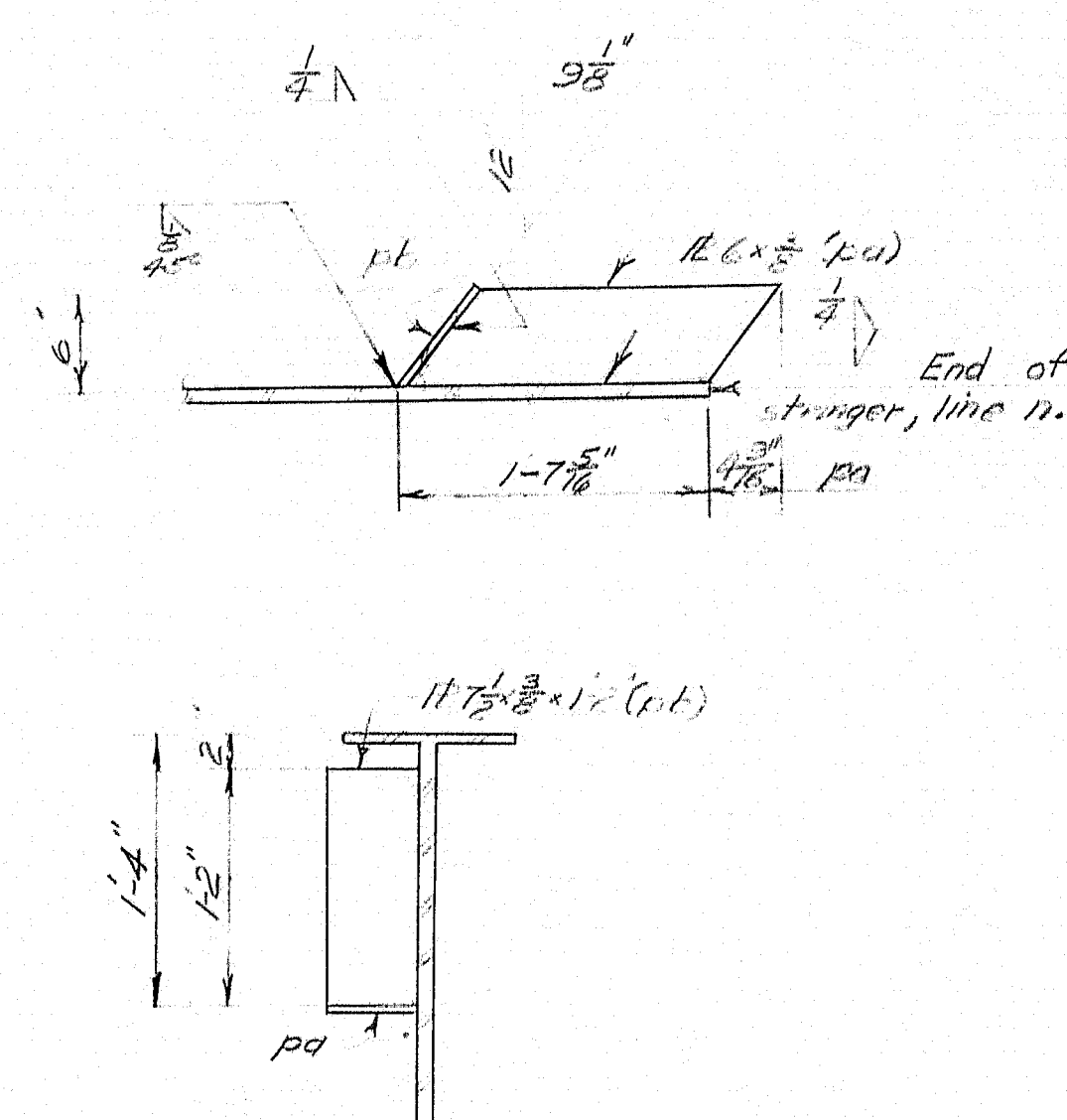


LONG CHORD LAYOUT

Stringers:
 S1 33 WF 130 6 Req'd.
 S2 33 WF 118 6 Req'd.
 S3 33 WF 130 6 Req'd.

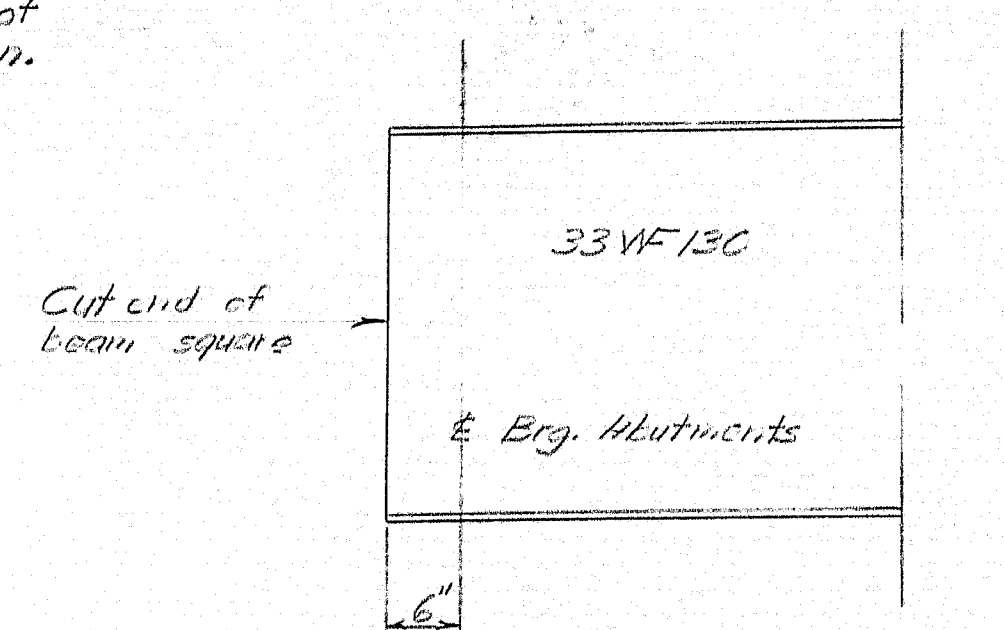
Diaphragms:
 D5 - 16 WF 36 10 Req'd.
 D6 - 15 C 33.9 11 Req'd.
 D7 - 16 WF 36 20 Req'd.
 D8 - 15 C 33.9 10 Req'd.

ERECTION DIAGRAM

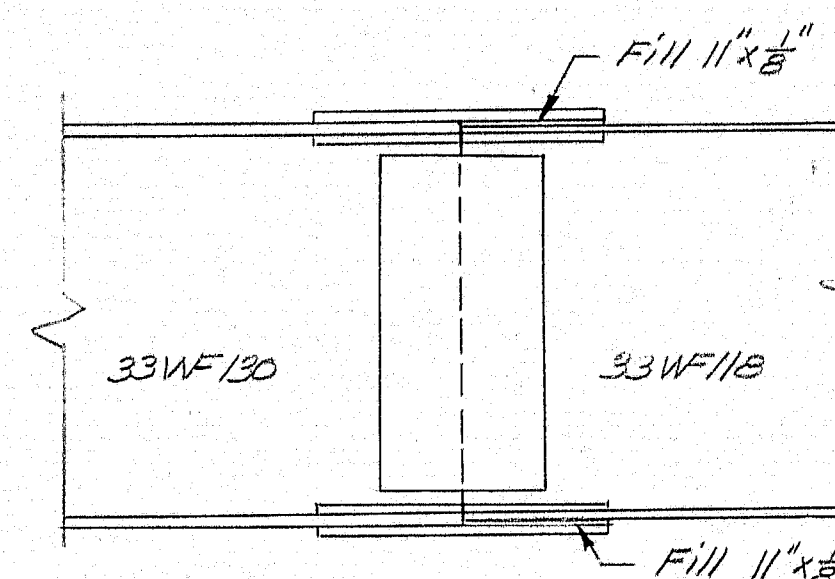


BRACKET PLATE DETAILS

Bracket plates in S3n shown.
 Bracket plates in S1n similar.



TYPICAL END OF STRINGER



TYPICAL SPLICE
 Standard Splice 33 WF 118

STRINGER GRADES IN PERCENT			
Line	S1	S2	S3
G	1.607	1.429	1.253
H	1.591	1.413	1.236
J	1.575	1.398	1.219
K	1.559	1.383	1.201
M	1.543	1.364	1.184
N	1.526	1.346	1.166

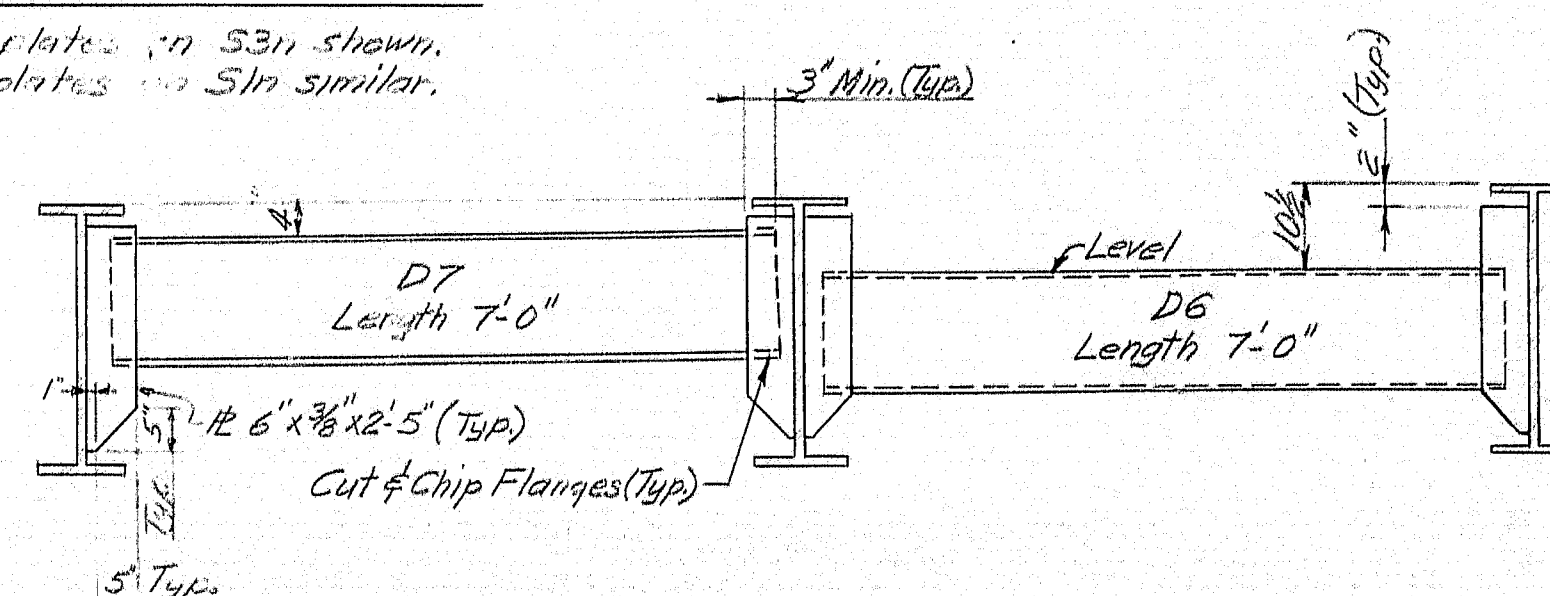
All grades are plus.

SPECIFICATIONS:
 Design - AASHTO Standard Specifications and revisions (1961).
 Fabrication & Erection - State of Maine Standard Specifications,
 Highway Bridges, Revision of Jan. 1956 with Supplemental
 Specifications.
 Materials - Steel stringers & splice plates shall be structural
 steel ASTM designation A36. All other structural steel
 shall be ASTM designation A7 or A36.

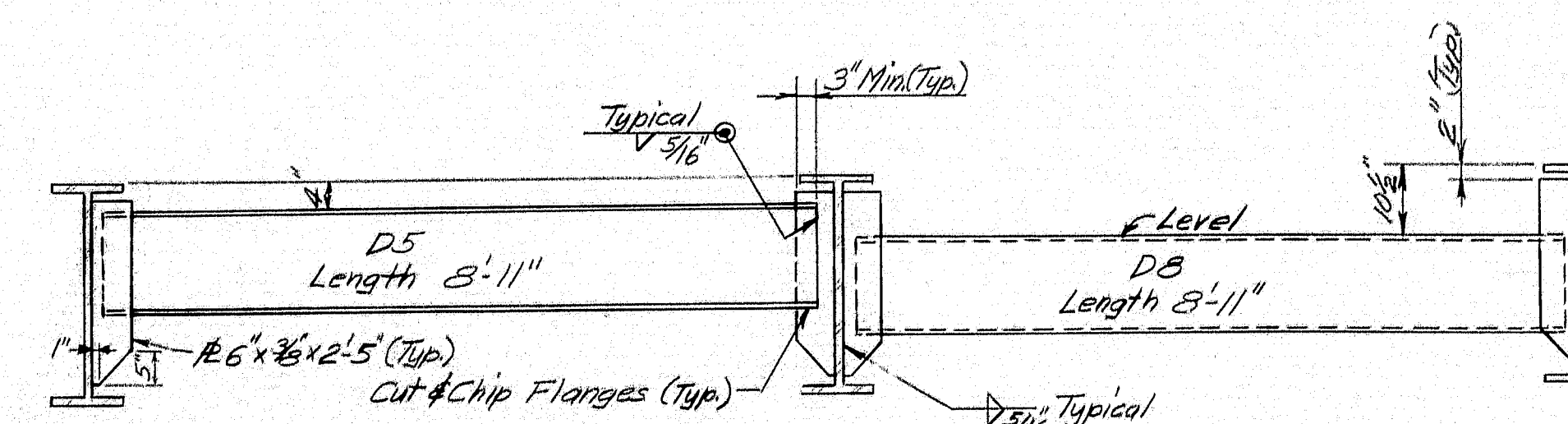
SPLICE NOTES:
 Fit the splice cut 33 WF 130 square and cut 33 WF 118 to match. Each
 row of bolts in the web splice shall be normal to the axis of 33 WF 130.
 Splice details are shown on Standard Details, E.D. 105-62, Beam Splices.
 Use standard splice for 33 WF 118.

BEARING NOTE:
 Bearing details are shown on Standard Details, P.D. 101-62, Bearing
 Pedestals. Use F.P.B.-2 at Abutments 1 & 2 and Pier 2. Use
 F.P.B.-2 at Pier 1.

GENERAL NOTES:
 Amayed Joints and Drains are shown on Sheet 22.
 No camber all stringers and no cover plates required.
 All dimensions are horizontal.
 All diaphragms shall be set plumb.



DIAPHRAGM DETAILS
 Similar to details for D7-4.
 See Sheet 21

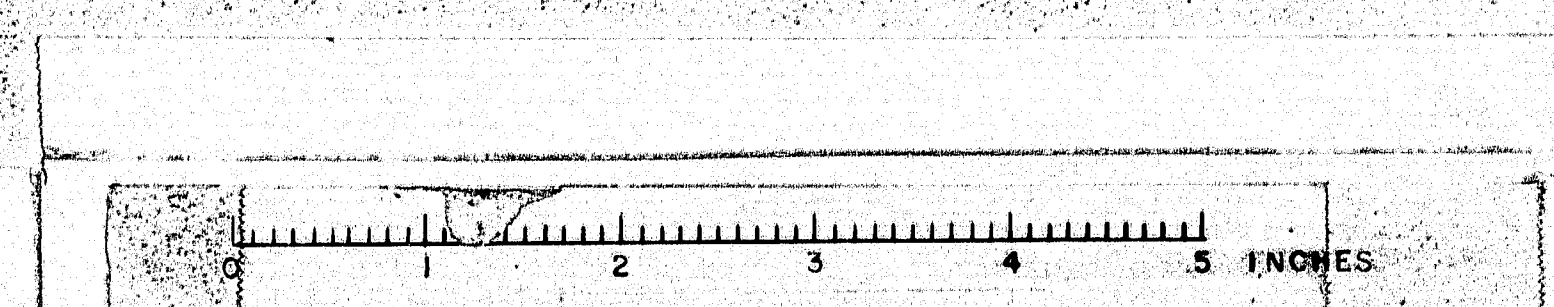


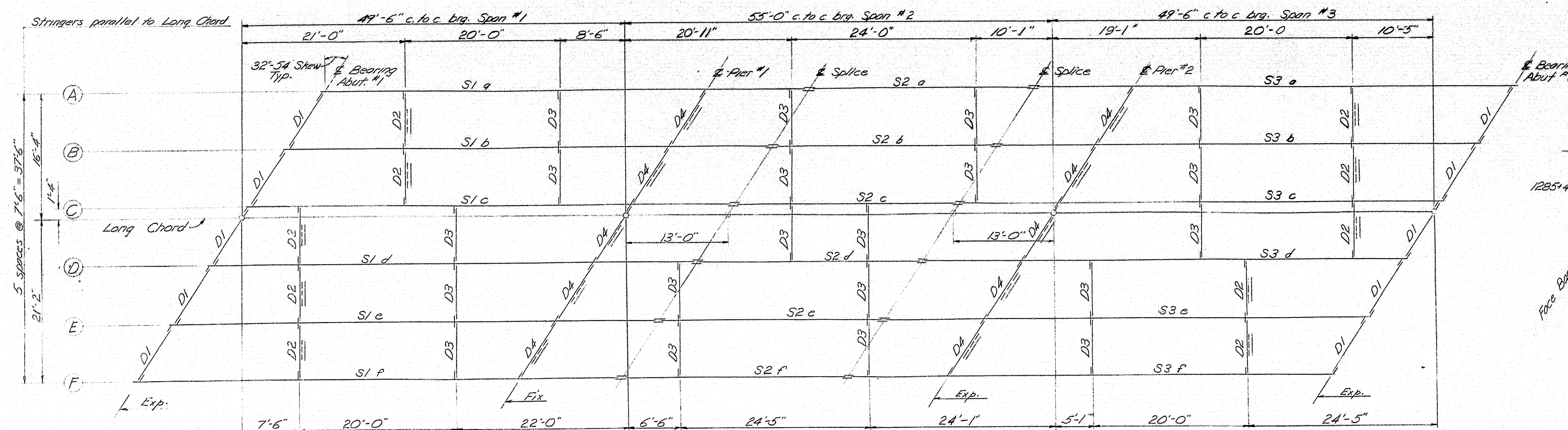
DESIGN-HRD
 TRACE-HRD
 CHECK-T.W.T.

STATE HIGHWAY COMMISSION
 BRIDGE DIVISION
 INTERSTATE 95 OVER RIVER ROAD
 IN THE TOWN OF
 BENTON
 KENNEBEC COUNTY
 STRUCTURAL STEEL S.B.

SHEET 20 OF 29 AUGUSTA MAINE JUNE 1963

(S.B.) Struct. Steel R. 90-20



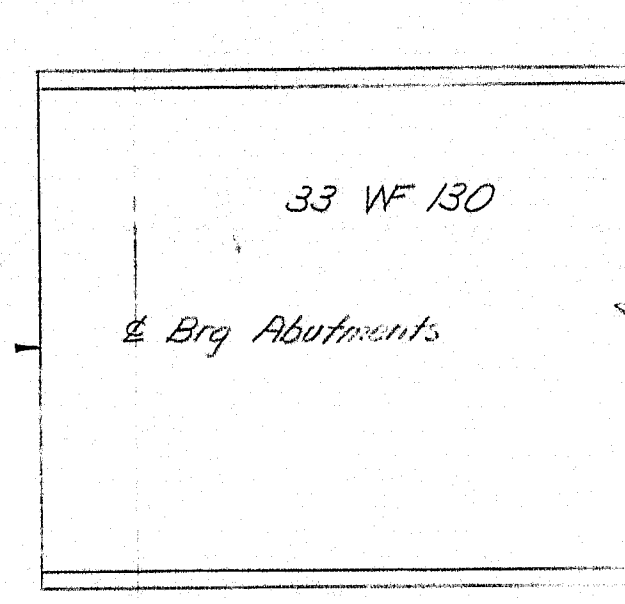


ERECTOR DIAGRAM

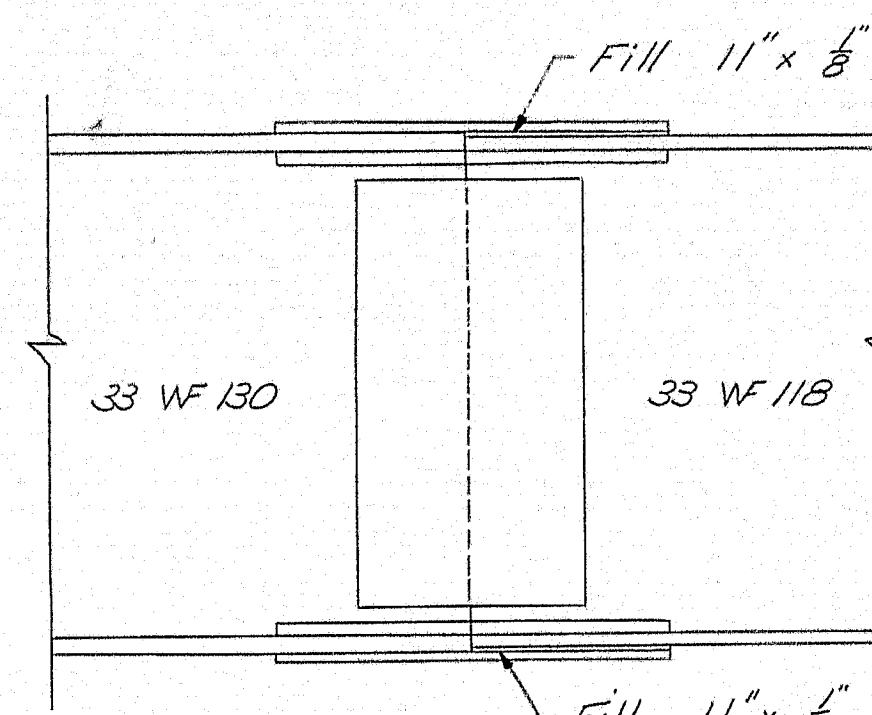
S1 - 33 WF 130 6 Reqd.
S2 - 33 WF 118 6 Reqd.
S3 - 33 WF 130 6 Reqd.

Diaphragms:

D1 - 16 WF 36 10 Reqd.
D2 - 15 L 33.9 10 Reqd.
D3 - 16 WF 36 20 Reqd.
D4 - 15 L 33.9 10 Reqd.



TYPICAL END OF STRINGER

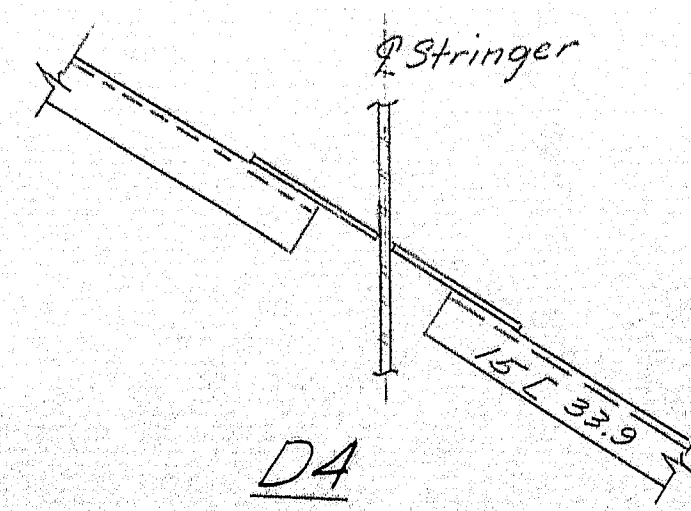
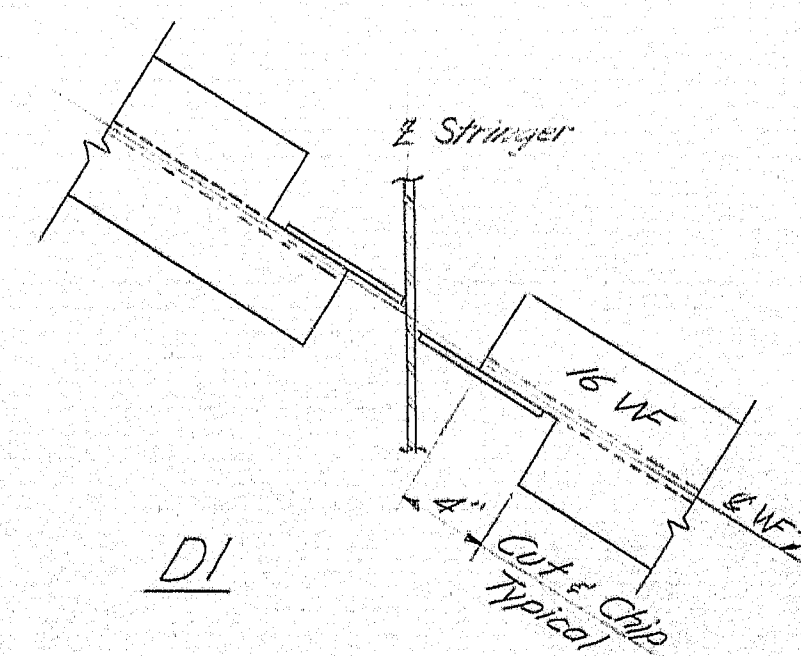
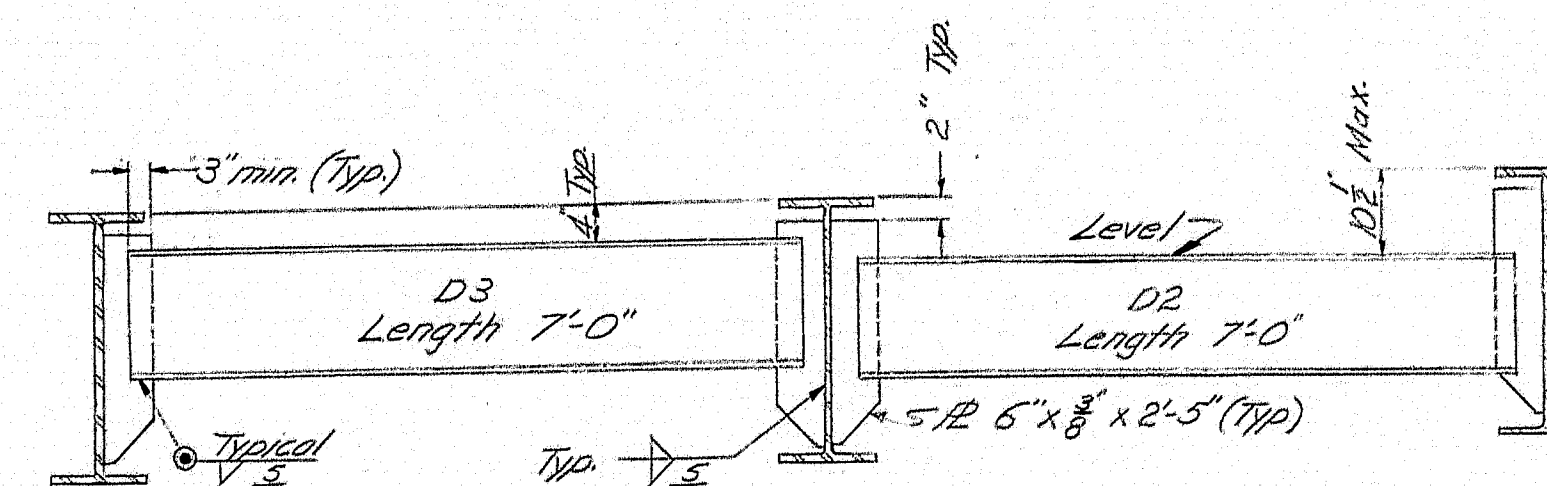
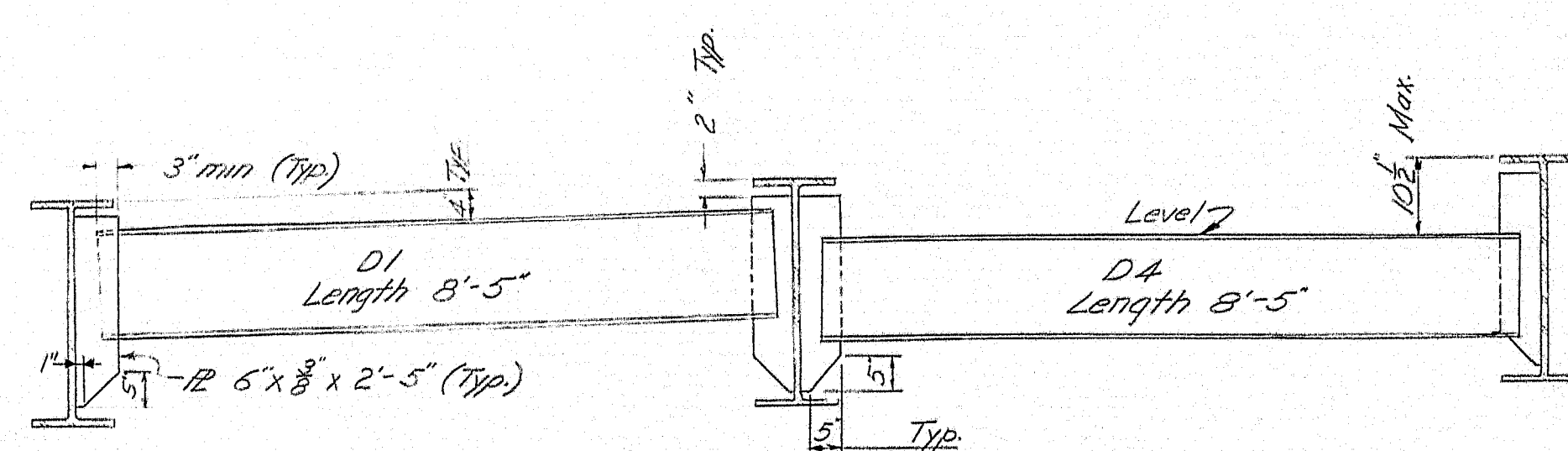


TYPICAL SPLICE
Standard splice 33 WF 118

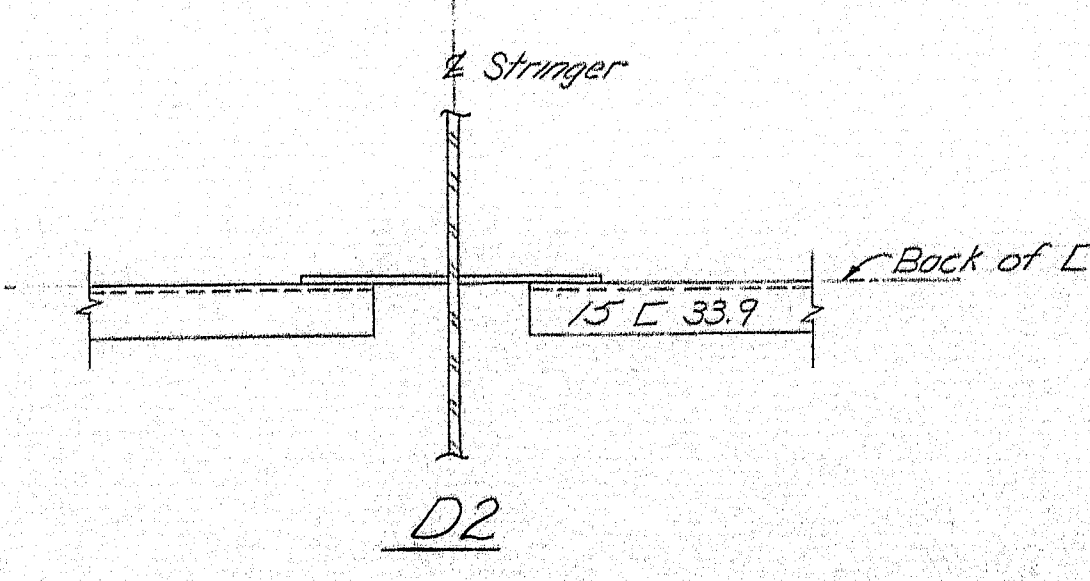
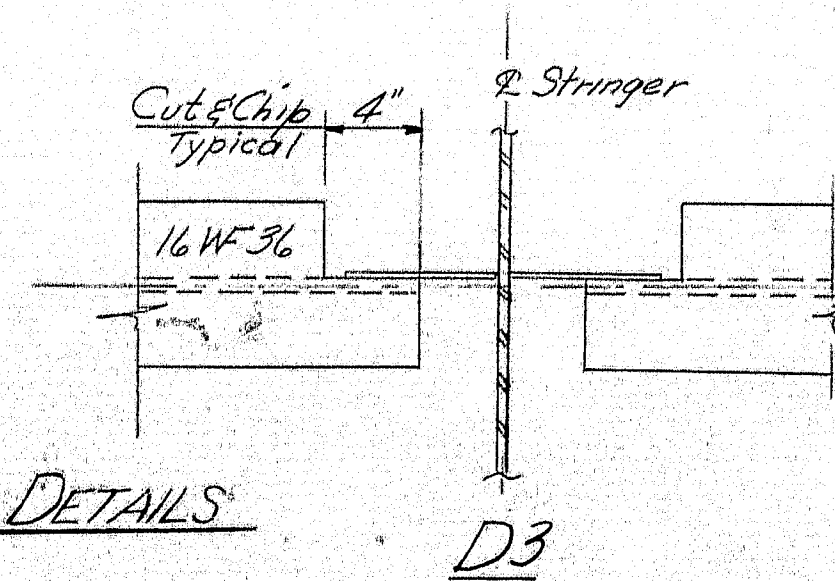
STRINGER GRADES IN PERCENT

Line	S1	S2	S3
a	1.5913	1.4393	1.2865
b	1.6027	1.4511	1.2988
c	1.6135	1.4631	1.3111
d	1.6248	1.4743	1.3232
e	1.6358	1.4860	1.3354
f	1.6465	1.4976	1.3473

All grades are plus



DIAPHRAGM DETAILS



SPECIFICATIONS:

Design - AASHTO Standard Specifications and revisions (1967)
Fabrication & Erection - State of Maine Standard Specifications, Highway Bridges, Revision of Jan. 1956 with Supplemental Specifications.
Materials - Steel stringers & splice plates shall be structural steel ASTM designation A36. All other structural steel shall be ASTM designation A7 or A36.

SPLICE NOTES:

At the splice cut 33 WF 130 square and cut 33 WF 118 to match. Each row of bolts in the web splice shall be normal to the axis of 33 WF 130.
Splice details are shown on "Structural Details", Form Splice, B10123-2. Use standard splice for 33 WF 118.

BEARINGS NOTE:

Bearing details are shown on "Structural Details", Bearing Pad, B10123-1. Use EAB-2 at Abutments 1 & 2 and Pier 2. Use FPB-2 at Pier 1.

GENERAL NOTES:

Armored Joints & Drains are shown on sheet #22.
No camber all stringers & no cover plates required.
All dimensions are horizontal.
All diaphragms shall be set plumb.

DESIGN AND CHECK: H.W.T.

DET-808

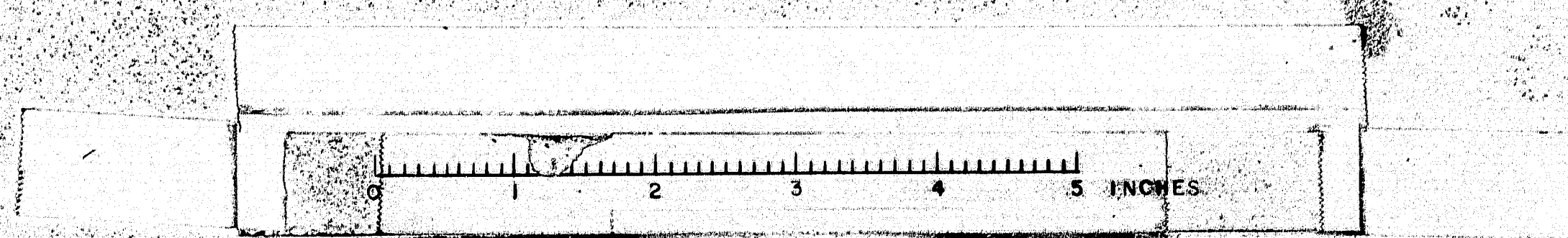
STATE HIGHWAY COMMISSION
BRIDGE DIVISION

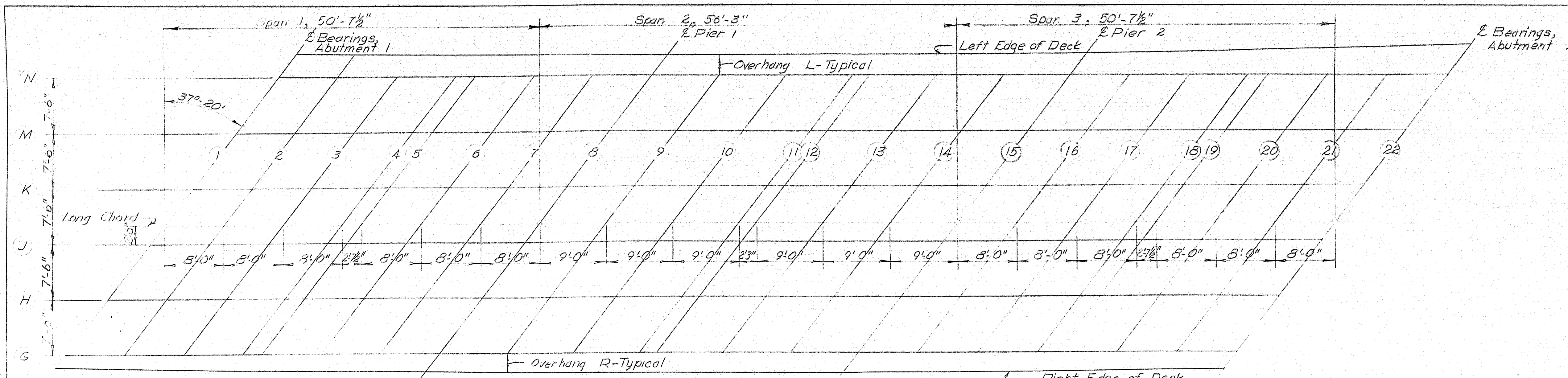
INTERSTATE 95 OVER RIVER ROAD
IN THE TOWN OF
BENTON
KENNEBEC COUNTY

STRUCTURAL STEEL N.B.

SHEET 21 OF 29 AUGUSTA MAINE JUNE 1963

(N.B.) STRUCT. STEEL 90-21



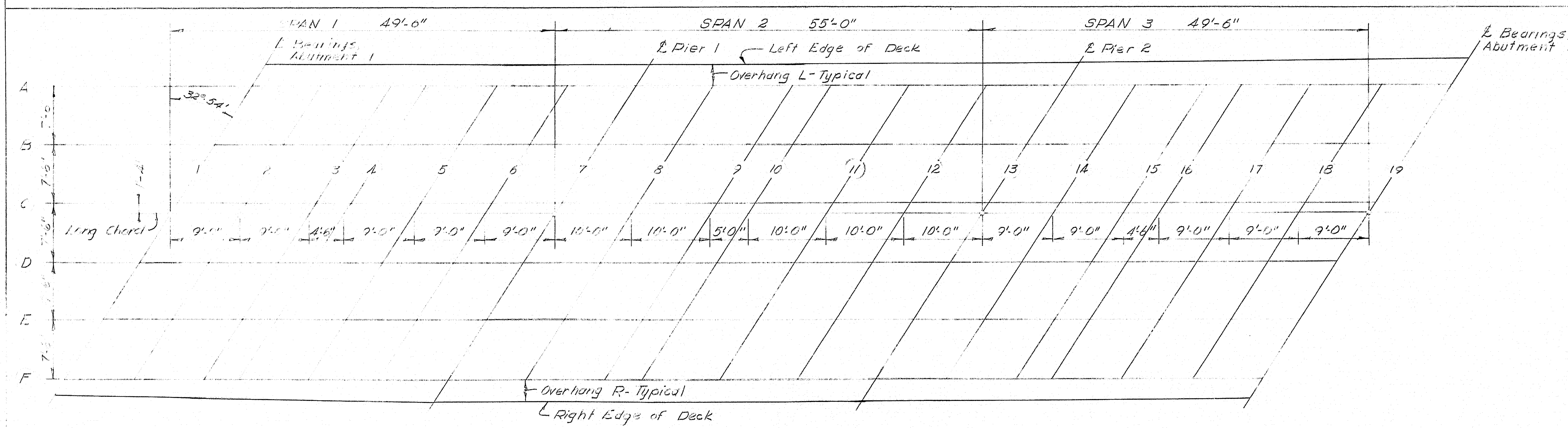


BLOCKING PLAN - SOUTHBOUND

BOTTOM OF SLAB ELEVATIONS & OVERHANG OF EDGE OF DECK																						
	1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16	17	18	19	20	21	22
Overhang L	3'-2 ¹³ / ₈ "	3'-1 ¹ / ₂ "	3'-0 ³ / ₈ "	2'-11 ¹ / ₈ "	2'-11 ¹ / ₈ "	2'-10 ³ / ₈ "	2'-9 ³ / ₈ "	2'-9 ³ / ₈ "	2'-9 ³ / ₈ "	2'-9 ³ / ₈ "	2'-9 ³ / ₈ "	2'-9 ³ / ₈ "	2'-10 ³ / ₈ "	2'-10 ³ / ₈ "	2'-11 ¹ / ₈ "	3'-0 ³ / ₈ "	3'-2 ¹ / ₈ "	3'-3 ³ / ₈ "	3'-4"	3'-5 ³ / ₈ "	3'-7 ³ / ₈ "	3'-9 ³ / ₈ "
Line N	142.15	142.23	142.37	142.49	142.53	142.64	142.75	142.86	142.99	143.12	143.25	143.28	143.40	143.50	143.62	143.72	143.83	143.94	143.97	144.06	144.13	144.21
" M	142.29	142.35	142.49	142.61	142.65	142.77	142.87	142.98	143.12	143.25	143.38	143.41	143.53	143.64	143.75	143.86	143.97	144.07	144.11	144.20	144.28	144.35
" K	142.32	142.47	142.61	142.73	142.77	142.89	143.00	143.11	143.24	143.38	143.51	143.54	143.66	143.77	143.89	144.00	144.11	144.21	144.25	144.34	144.42	144.49
" J	142.44	142.58	142.72	142.85	142.89	143.01	143.12	143.23	143.37	143.51	143.64	143.67	143.79	143.90	144.02	144.13	144.24	144.35	144.38	144.48	144.56	144.64
" H	142.55	142.70	142.84	142.97	143.01	143.13	143.24	143.36	143.49	143.63	143.77	143.80	143.92	144.03	144.15	144.26	144.38	144.49	144.52	144.62	144.70	144.78
" G	142.67	142.82	142.96	143.09	143.13	143.25	143.36	143.48	143.62	143.76	143.89	143.93	144.05	144.17	144.29	144.40	144.52	144.62	144.66	144.76	144.84	144.92
Overhang R	1'-9 ³ / ₈ "	1'-11 ¹ / ₈ "	2'-0 ³ / ₈ "	2'-2 ³ / ₈ "	2'-2 ³ / ₈ "	2'-4 ¹ / ₂ "	2'-5 ³ / ₈ "	2'-6 ³ / ₈ "	2'-7 ³ / ₈ "	2'-7 ³ / ₈ "	2'-8 ³ / ₈ "	2'-8 ³ / ₈ "	2'-8 ³ / ₈ "	2'-8 ³ / ₈ "	2'-8 ⁷ / ₈ "	2'-8"	2'-7 ³ / ₈ "	2'-6 ³ / ₈ "	2'-5 ³ / ₈ "	2'-4 ¹ / ₂ "	2'-2 ¹⁵ / ₁₆ "	

BLOCKING DETAIL
Northbound and Southbound

Note:
In order that the roadway slab will conform to the profile and cross sections shown on these plans the accompanying table of Elevations is given. Elevations for the bottom of the slab, which are computed to compensate for dead load deflections, must be set before slab forms are started.

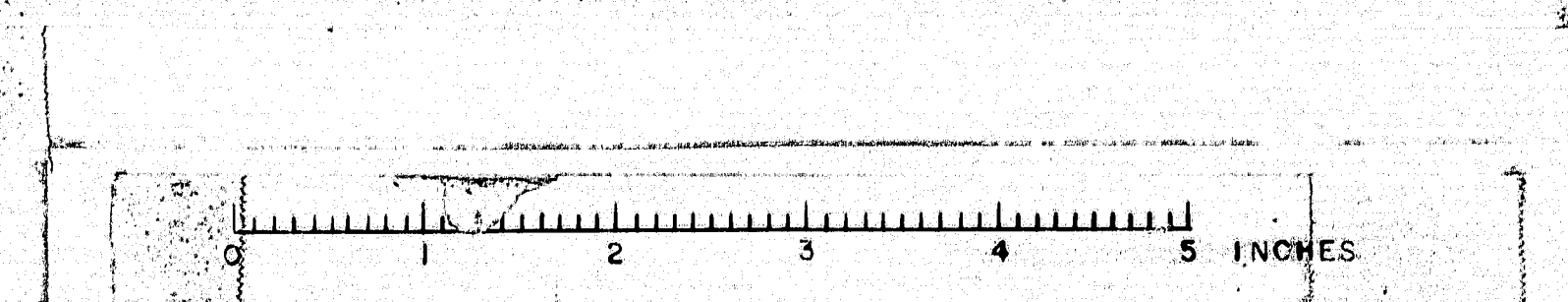


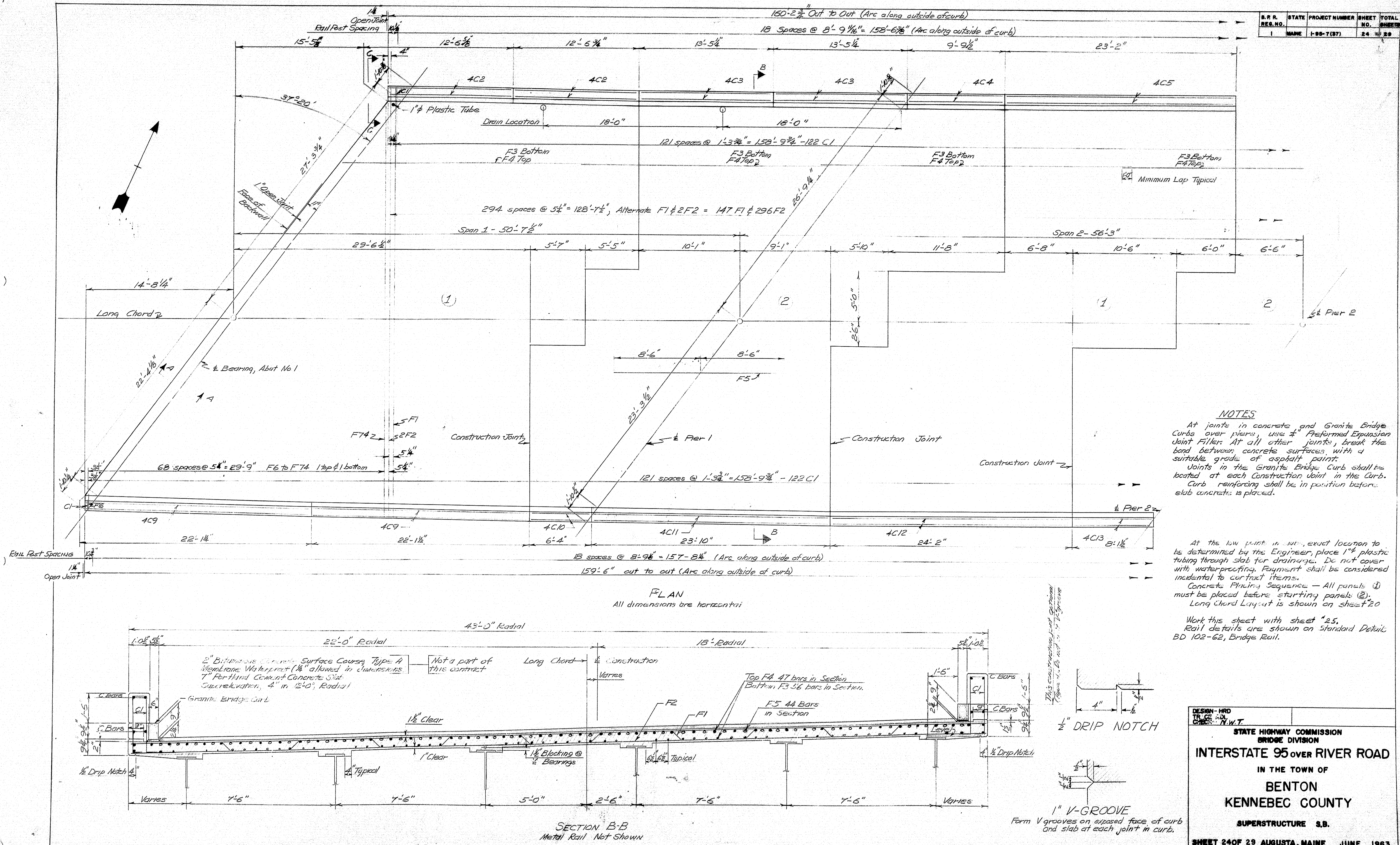
BLOCKING PLAN - NORTHBOUND

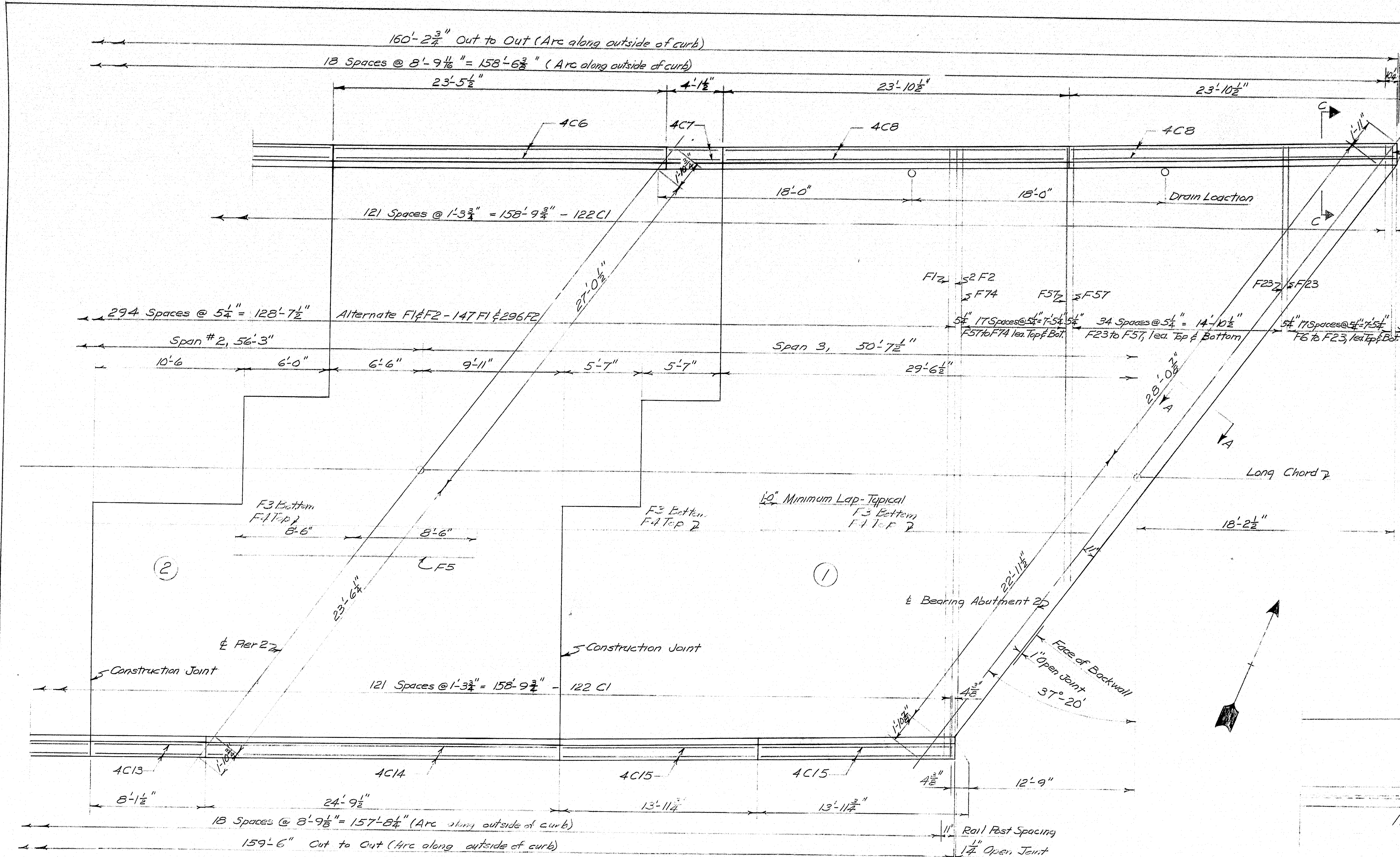
BOTTOM of SLAB ELEVATIONS & OVERHANG of EDGE of DECK																			
	1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16	17	18	19
Overhang L	2'-11 ¹³ / ₈ "	2'-10 ³ / ₈ "	2'-9 ¹ / ₈ "	2'-8 ⁹ / ₈ "	2'-7 ⁵ / ₈ "	2'-6 ³ / ₈ "	2'-6 ³ / ₈ "	2'-6 ¹ / ₈ "	2'-6"	2'-6 ¹ / ₈ "	2'-6 ⁷ / ₈ "	2'-7 ¹ / ₈ "	2'-7 ³ / ₈ "	2'-8 ⁵ / ₈ "	2'-10 ¹ / ₈ "	2'-10 ¹³ / ₈ "	3'-0 ³ / ₈ "	3'-2 ⁵ / ₈ "	3'-4 ¹ / ₈ "
Line A	149.12	149.25	149.44	149.51	149.65	149.78	149.91	150.06	150.21	150.29	150.43	150.56	150.70	150.83	150.96	151.02	151.14	151.24	151.34
" B	149.25	149.42	149.57	149.65	149.78	149.91	150.04	150.20	150.35	150.43	150.57	150.70	150.84	150.97	151.10	151.17	151.28	151.39	151.48
" C	149.33	149.55	149.70	149.78	149.91	150.04	150.18	150.33	150.49	150.56	150.71	150.84	150.98	151.11	151.24	151.31	151.43	151.53	151.63
" D	149.51	149.67	149.83	149.91	150.05	150.18	150.31	150.47	150.62	150.70	150.85	150.98	151.12	151.25	151.39	151.45	151.57	151.68	151.78
" E	149.63	149.80	149.96	150.04	150.18	150.31	150.44	150.60	150.76	150.84	150.98	151.12	151.26	151.39	151.53	151.60	151.72	151.82	151.92
" F	149.76	149.93	150.09	150.17	150.31	150.44	150.58	150.74	150.90	150.97	151.12	151.26	151.40	151.54	151.67	151.74	151.86	151.97	152.07
Overhang R	2'-1 ¹ / ₄ "	2'-3 ³ / ₈ "	2'-5 ³ / ₈ "	2'-6"	2'-7 ⁷ / ₈ "	2'-8 ³ / ₈ "	2'-9 ¹³ / ₈ "	2'-10 ¹ / ₈ "	2'-11 ³ / ₈ "	2'-11 ⁵ / ₈ "	2'-11 ⁵ / ₈ "	3'-0"	2'-11 ³ / ₈ "	2'-11 ³ / ₈ "	2'-10 ¹ / ₈ "	2'-10 ⁵ / ₈ "	2'-9 ⁵ / ₈ "	2'-8 ³ / ₈ "	2'-6 ¹ / ₂ "

DES-AMR & MRO DET-EB
TRACE-EB
CHECK-N.W.T.

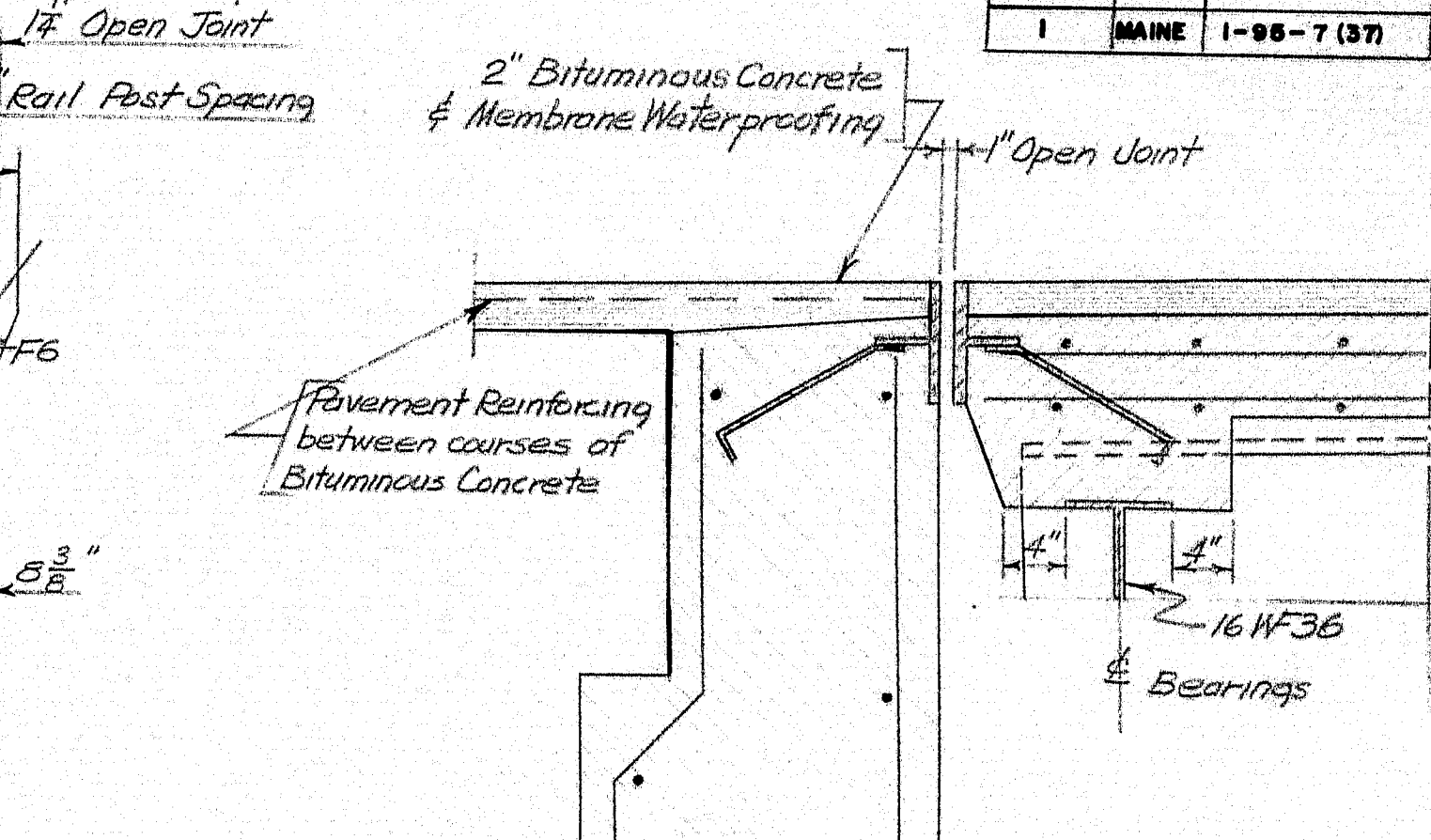
STATE HIGHWAY COMMISSION
BRIDGE DIVISION
INTERSTATE 95 OVER RIVER ROAD
IN THE TOWN OF
BENTON
KENNEBEC COUNTY
BLOCKING SB & NB
SHEET 23 OF 29 AUGUSTA MAINE JUNE 1963





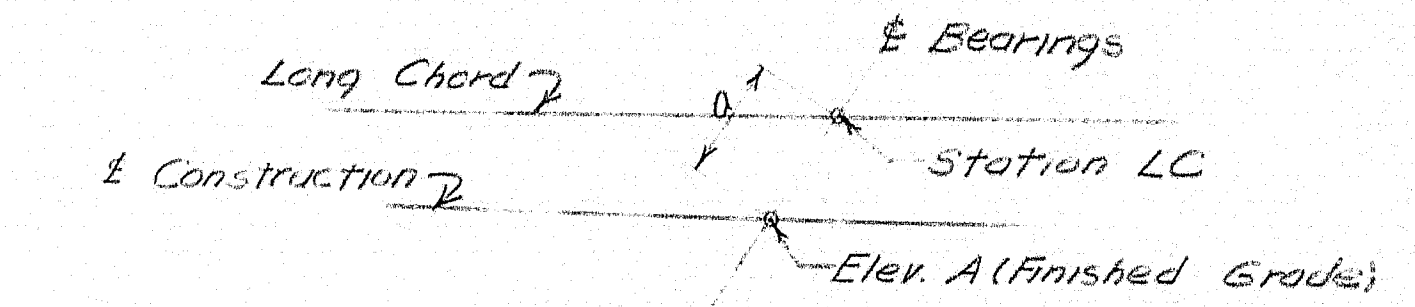


PLAN
All dimensions are horizontal

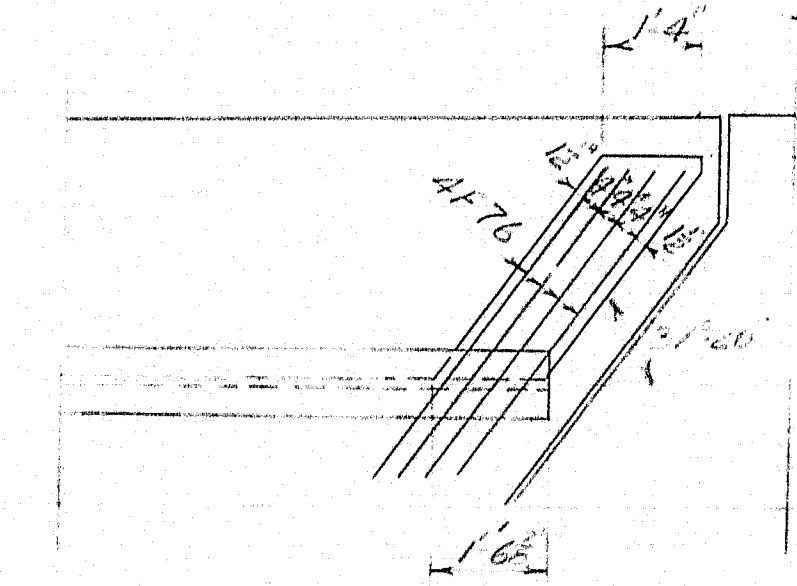


SECTION A-A

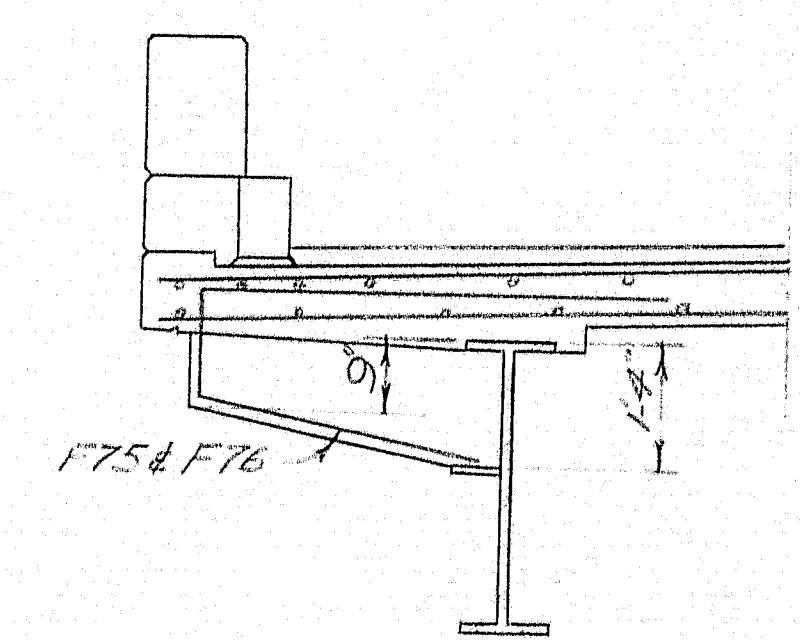
NOTE: Pavement Reinforcing Details - 36" minimum width x 30'-0"±, 4"x4"-10 gage galvanized welded wire mesh. (An equivalent, satisfactory to the Engineer, may be substituted if 44-1010 is not readily available.) Bituminous Concrete, Membrane Waterproofing, and Pavement Reinforcing are not a part of this Contract.



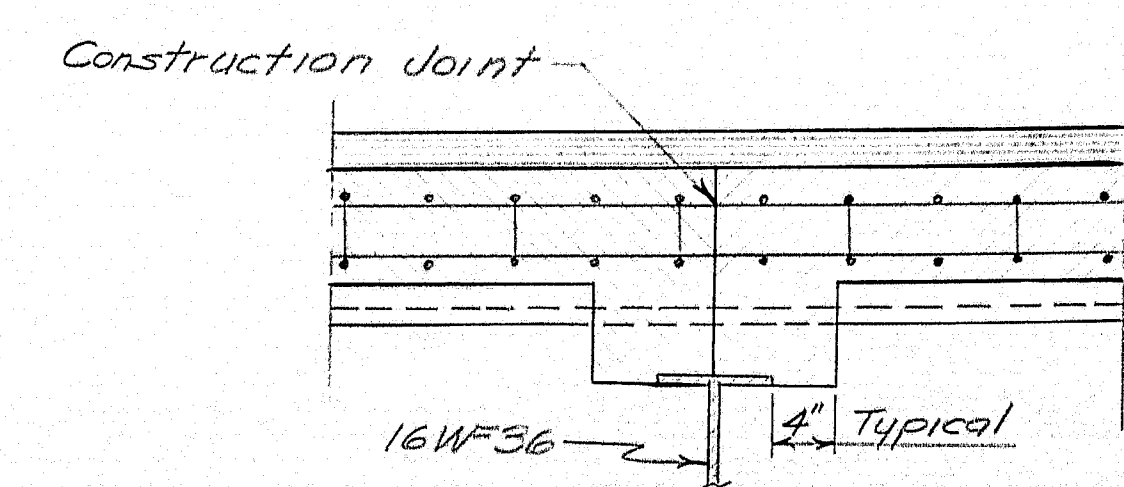
± BEARINGS	STATION LC	A	a
Abutment 1	1287 + 01.03	143.17	9 3/8"
± Pier 1	1287 + 51.66	143.93	9 3/8"
± Pier 2	1288 + 07.91	144.76	9 3/8"
Abutment 2	1288 + 58.53	145.36	9 3/8"



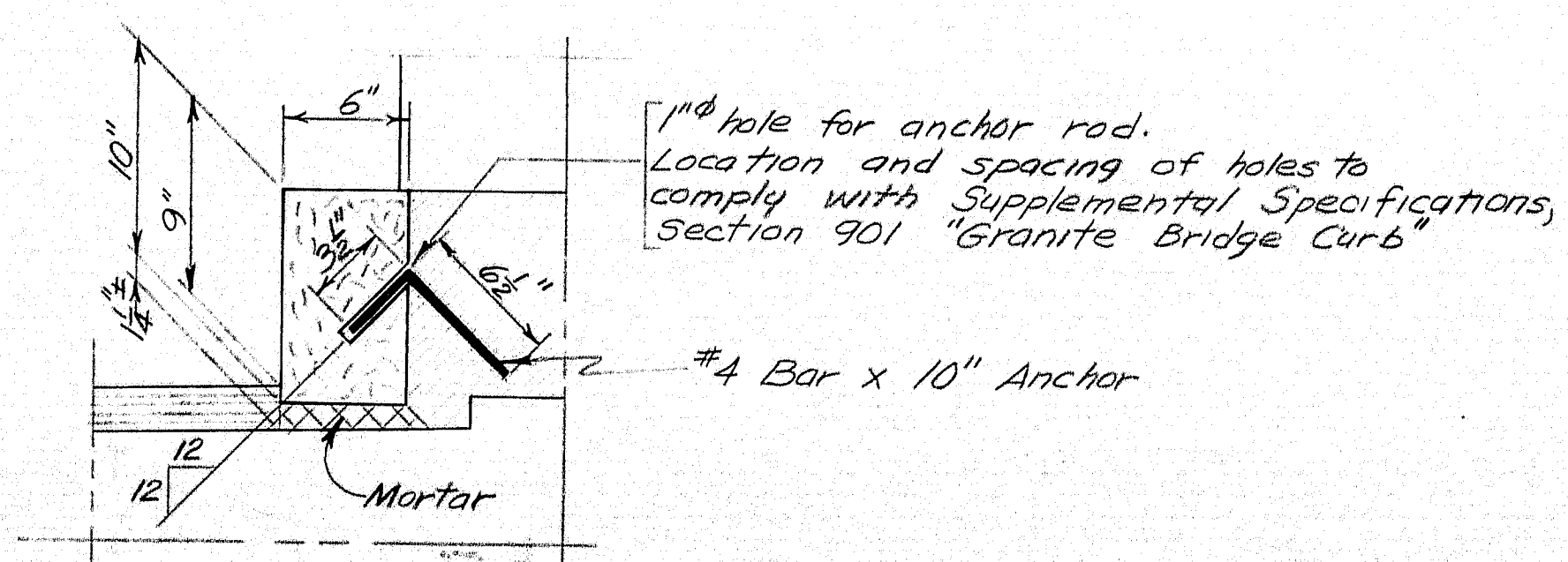
BRACKET PLAN, SPAN 3



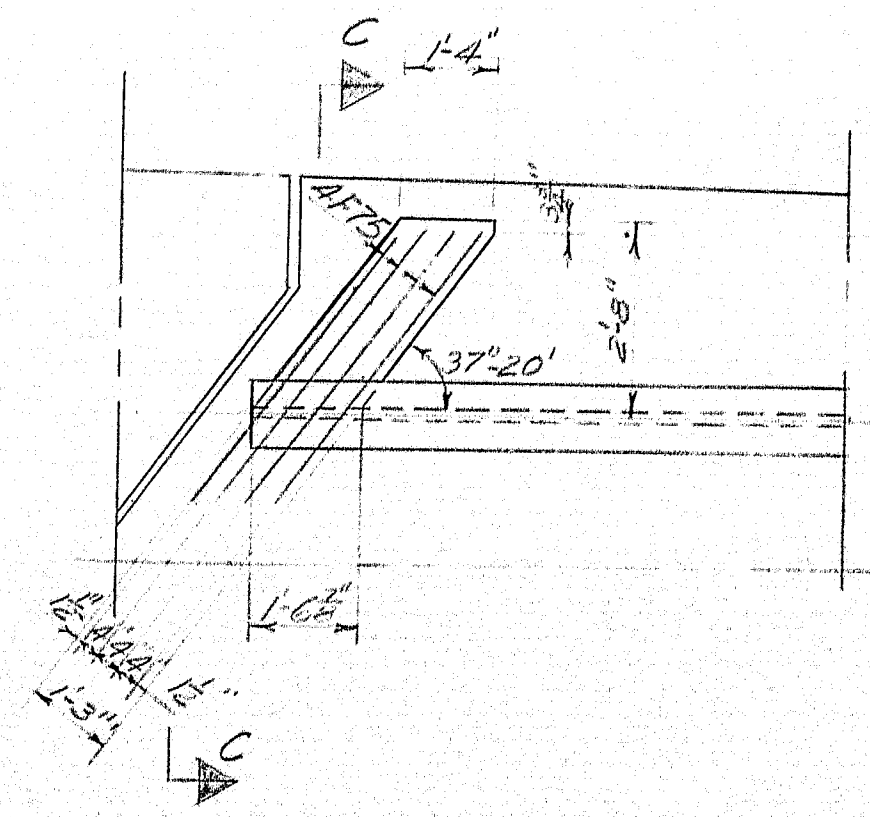
SECTION CC



TYPICAL SLAB CONSTRUCTION JOINT



TYPICAL GRANITE BRIDGE CURB



BRACKET PLAN, SPAN 1

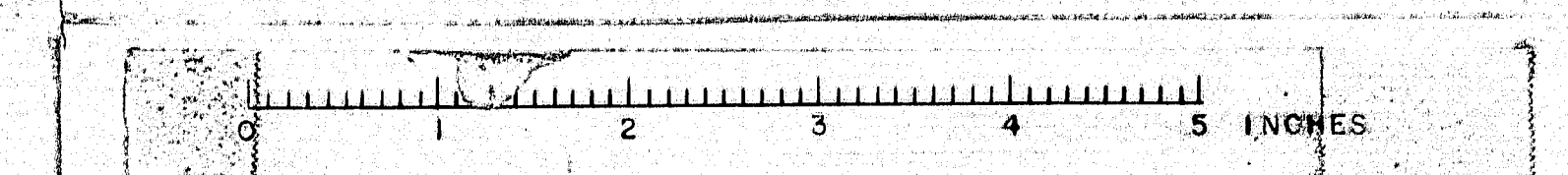
DESIGN: HPD
TRACE: JDL
CHECK: N.W.T.

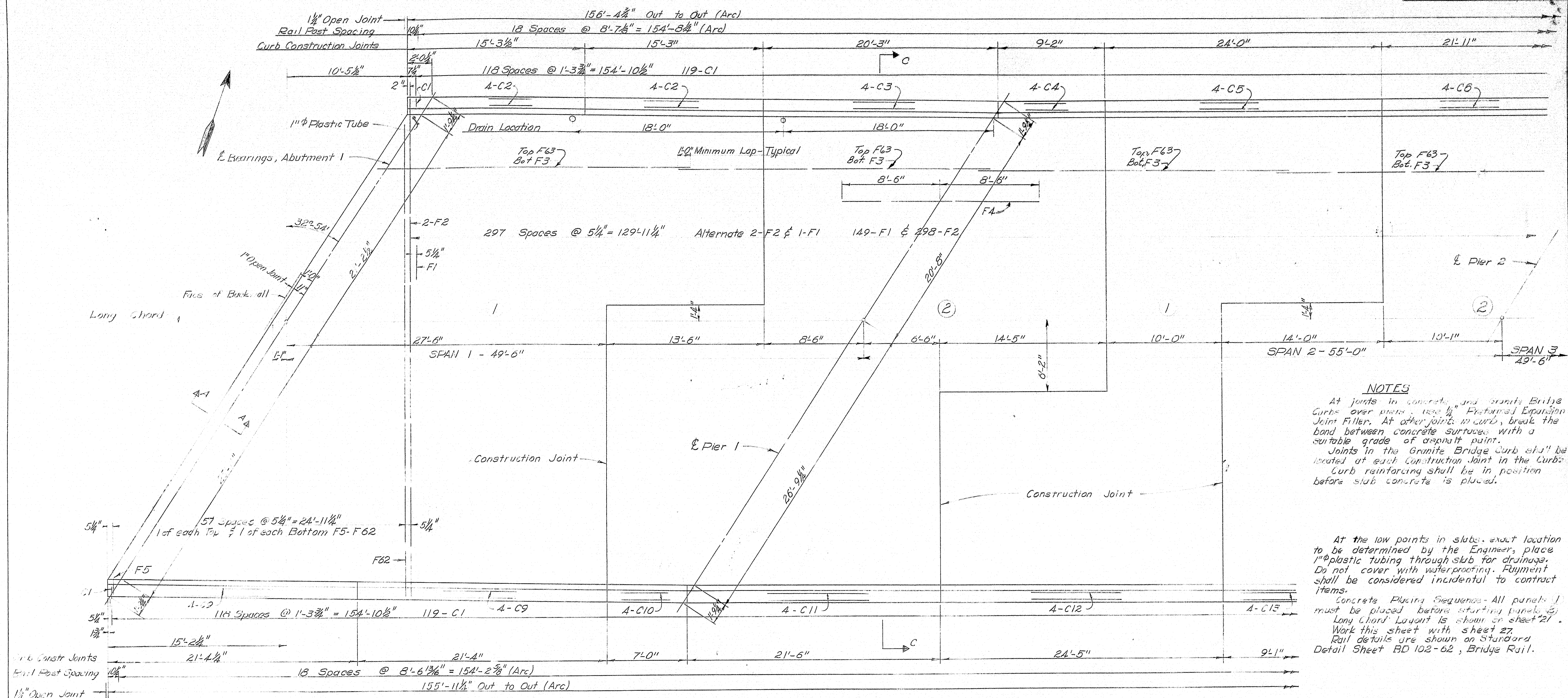
STATE HIGHWAY COMMISSION
BRIDGE DIVISION

INTERSTATE 95 OVER RIVER ROAD
IN THE TOWN OF
BENTON
KENNEBEC COUNTY

SUPERSTRUCTURE S.B.

SHEET 25 OF 29 AUGUSTA, MAINE, JUNE 1963





NOTES

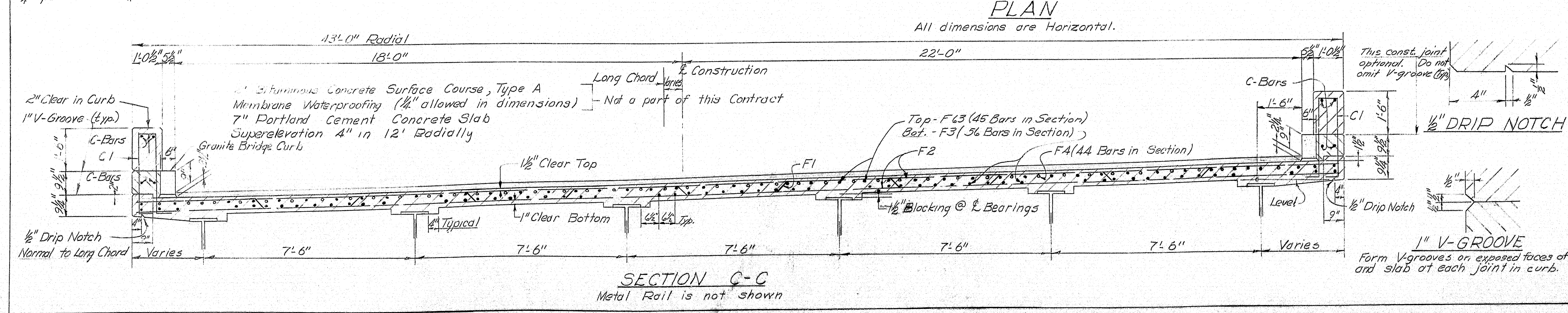
At joints in concrete and granite Bridge Curb over spans, use 1/2" Extruded Expansion Joint Filler. At other joints in curb, break the bond between concrete surfaces with a suitable grade of asphalt paint.

Joints in the Granite Bridge Curb shall be located at each Construction Joint in the Curb. Curb reinforcing shall be in position before slab concrete is placed.

At the low points in slabs, exact location to be determined by the Engineer, place 1" plastic tubing through slab for drainage. Do not cover with waterproofing. Payment shall be considered incidental to contract items.

Concrete Placing Sequence - All panels must be placed before starting panels 25. Long Chord Layout is shown on sheet 27. Work this sheet with sheet 27.

Rail details are shown on Standard Detail Sheet BD 102-62, Bridge Rail.

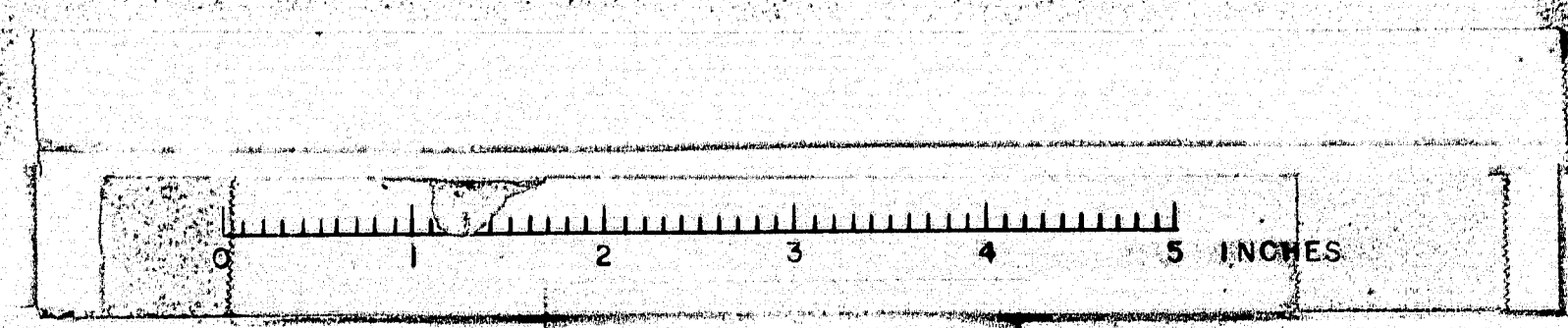


DESIGN - AHR
TRACE - EB
CHECK - N.W.T.

STATE HIGHWAY COMMISSION
BRIDGE DIVISION
INTERSTATE 95 OVER RIVER ROAD
IN THE TOWN OF
BENTON
KENNEBEC COUNTY
SUPERSTRUCTURE N.B.

SHEET 26 OF 29 AUGUSTA MAINE JUNE 1963

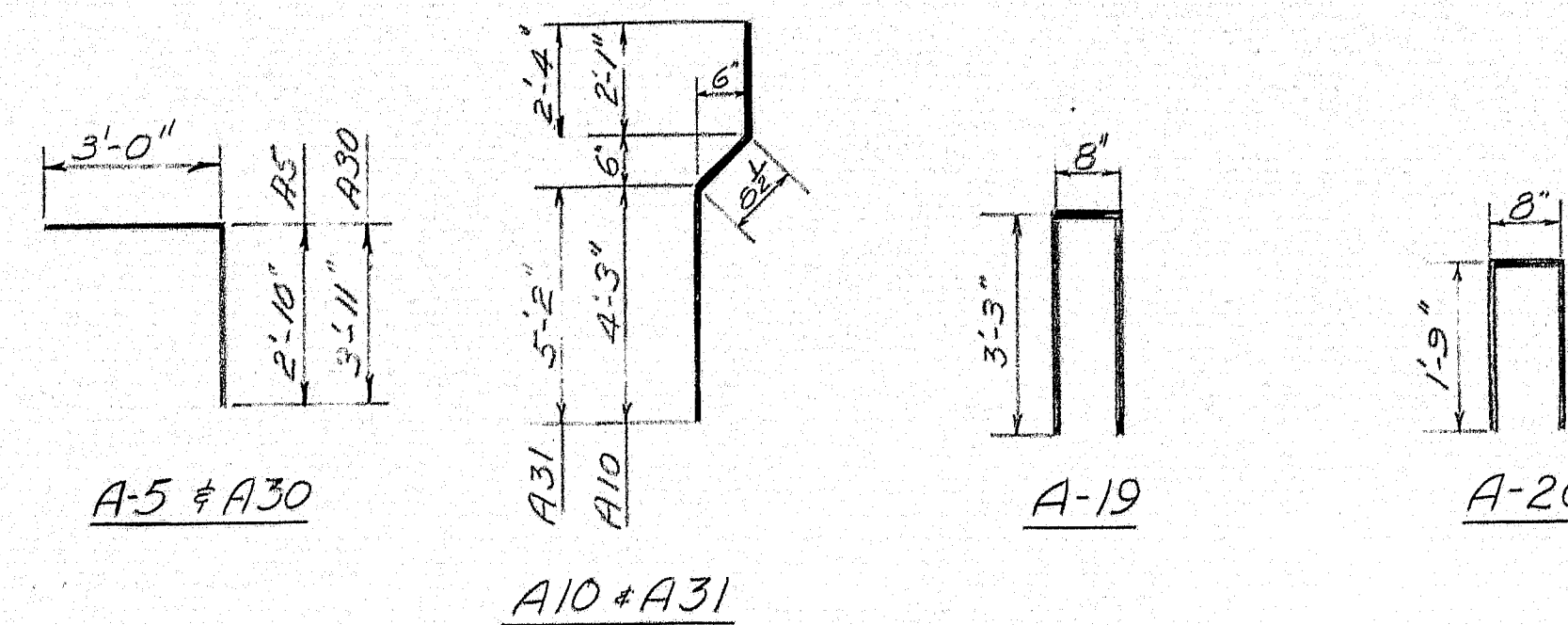
Supra M.B. River Rd. - Benton 90-26



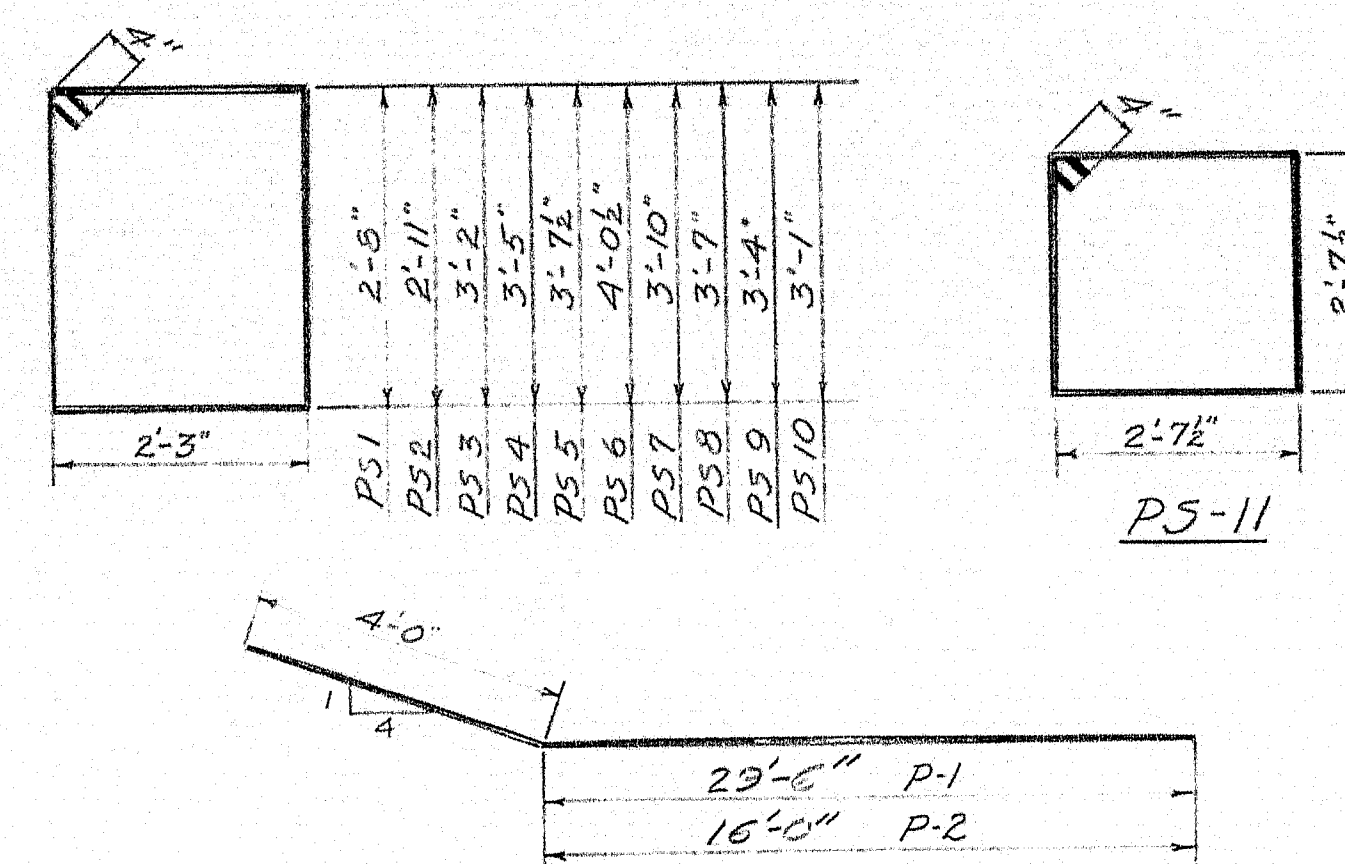
REINFORCING STEEL SCHEDULE - SOUTH BOUND

STATE PROJECT NUMBER SHEET TOTAL
1 MAINE 1-95-7(37) 28 29

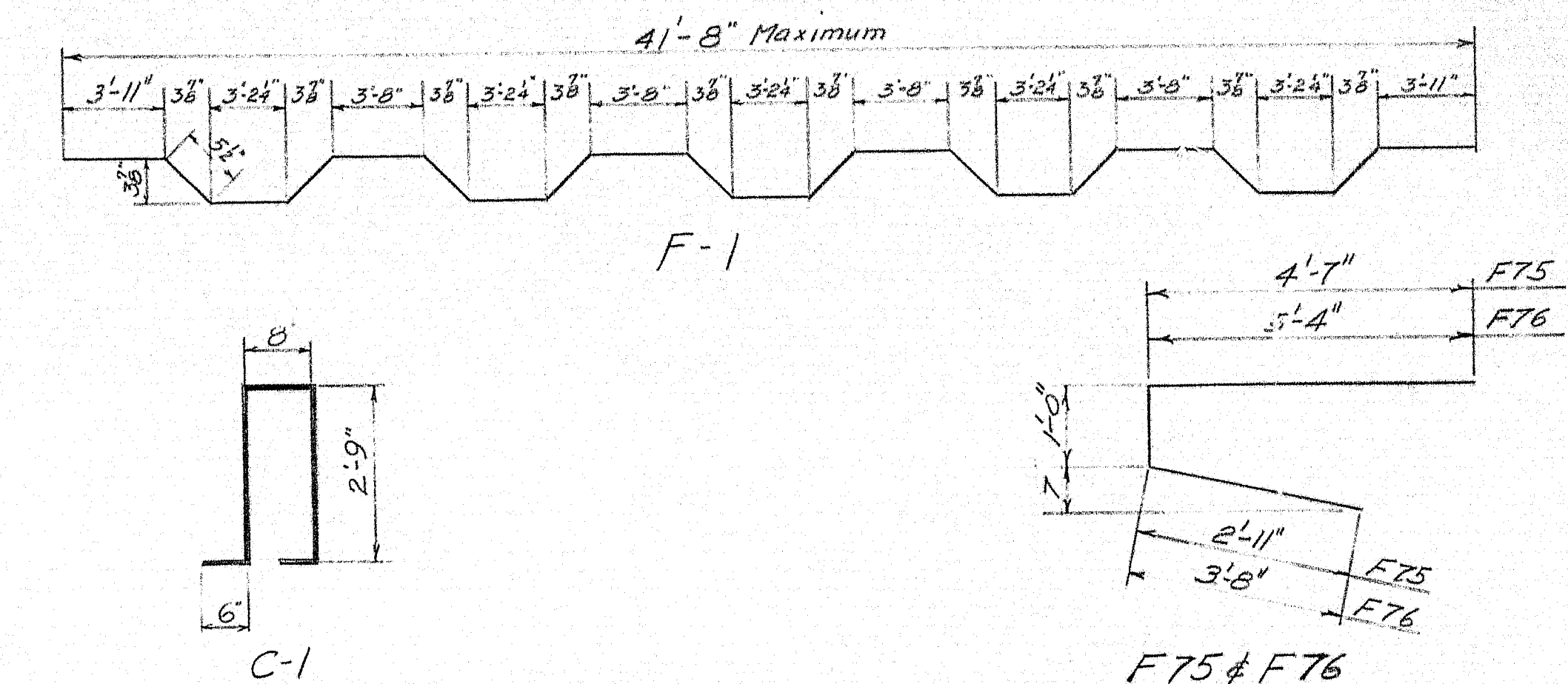
ABUTMENTS



PIERS



SUPERSTRUCTURE



ABUTMENT #1					ABUTMENT #2					PIER #1					PIER #2					SUPERSTRUCTURE				
BENT BARS					BENT BARS					BENT BARS					BENT BARS					BENT BARS				
MARK	SIZE	NUMBER	LENGTH	LOCATION	MARK	SIZE	NUMBER	LENGTH	LOCATION	MARK	SIZE	NUMBER	LENGTH	LOCATION	MARK	SIZE	NUMBER	LENGTH	LOCATION	MARK	SIZE	NUMBER	LENGTH	LOCATION
A5	#5	34	5'-10"	Breastwall	A19	#4	20	7'-2"	Wing Walls	PS1	#4	2	10'-6"	Pier Cap	PS1	#4	2	10'-6"	Pier Cap	F1	#5	147	43'-0"	Slab, Trans.
A10	#5	33	7'-0"	Backwall	A20	#4	6	4'-2"	End Posts	PS2	#4	2	11'-6"		PS2	#4	2	11'-6"		F75	#5	4	8'-6"	Slab Bracket, Span 1
A19	#4	20	7'-2"	Wing Walls	A30	#5	34	5'-11"	Breastwall	PS3	#4	2	12'-0"		PS3	#4	2	12'-0"		F76	#5	4	10'-0"	Slab Bracket, Span 3
A20	#4	6	4'-2"	End Posts	A31	#5	34	5'-2 1/2"	Backwall	PS4	#4	2	12'-0"		PS4	#4	2	12'-0"						
STRAIGHT BARS					STRAIGHT BARS					STRAIGHT BARS					STRAIGHT BARS					STRAIGHT BARS				
A1	#6	102	5'-1"	Footings	A1	#6	104	5'-1"	Footings	PS5	#4	48	12'-5"		PS5	#4	48	12'-5"		F2	#5	296	42'-8"	Slab Trans.
A2	#6	12	24'-0"		A4	#5	68	3'-0"		PS6	#4	40	13'-3"		PS6	#4	40	13'-3"		F3	#5	336	27'-5"	Slab Trans.
A3	#6	12	29'-0"		A6	#5	3	25'-6"	Bridge Seat	PS7	#4	2	12'-10"		PS7	#4	2	12'-10"		F4	#5	282	27'-5"	Slab Trans.
A4	#5	67	3'-0"		A7	#5	3	28'-0"		PS8	#4	2	12'-4"		PS8	#4	2	12'-4"		F5	#5	88	17'-0"	over piers
A6	#5	17	25'-6"	Breastwall & Backwall	A11	#5	33	4'-9"	Backwall	PS9	#4	2	11'-10"		PS9	#4	2	11'-10"		F6	#5	4	1'-5"	Trans. Ends
A7	#5	17	28'-0"		A12	#6	34	3'-6"	Slab Seat	PS10	#4	2	11'-4"		PS10	#4	2	11'-4"		F7	#5	4	2'-2"	
					A13	#6	52	2'-6"	Fig., Both Wings	P1	#8	7	33'-6"	Btm. Pier Cap	P1	#8	7	33'-6"	Btm. Pier Cap	F8	#5	4	2'-2"	
					A14	#6	8	11'-8"	Both Wings	P2	#8	7	20'-0"		P2	#8	7	20'-0"		F9	#5	4	3'-3"	
					A15	#5	42	2'-4"	Both Wings	STRAIGHT BARS					STRAIGHT BARS					F10	#5	4	3'-10"	
					A17	#5	10	11'-1"	Northerly Wing	P3	#6	8	26'-6"	Sides Pier Cap	P3	#6	8	26'-6"	Sides Pier Cap	F11	#5	4	4'-5"	
					A18	#5	20	9'-8"	Both Wings	P5	#8	8	26'-6"	Top Pier Cap	P5	#8	8	26'-6"	Top Pier Cap	F12	#5	4	5'-0"	
					A21	#4	4	9'-8"		P6	#9	6	26'-6"		P6	#9	6	26'-6"		F13	#5	4	5'-7"	
					A22	#4	4	2'-2"		STRAIGHT BARS					STRAIGHT BARS					F14	#5	4	6'-2"	
					A23	#5	20	9'-8"	Both Wings	P11	#6	48	5'-6"	Footings	P11	#6	48	5'-6"	Footings	F15	#5	4	6'-9"	
					A24	#5	2	2'-10"	Northerly Wing	P22	#8	16	15'-6"	Dowels, Fig.	P22	#8	16	15'-6"	Dowels, Fig.	F16	#5	4	7'-4"	
					A25	#5	8	12'-3"		P23	#8	16	16'-6"	Columns	P23	#8	16	16'-6"	Columns	F17	#5	4	7'-11"	
					A27	#6	8	13'-1"	Footings, Northerly Wing	P24	#8	16	18'-6"		P24	#8	16	18'-6"		F18	#5	4	8'-5"	
					A28	#5	20	8'-10"		P25	#8	16	17'-6"		P25	#8	16	17'-6"		F19	#5	4	9'-0"	
					A32	#5	14	25'-0"	Breastwall & Backwall	STRAIGHT BARS (CONT)					STRAIGHT BARS (CONT)					F20	#5	4	9'-7"	
					A33	#5	14	29'-0"		C1	#4	244	7'-2"	Curb Straps	C1	#4	8	23'-7"	Curb (long)	F21	#5	4	10'-1"	
					A34	#6	12	25'-3"	Footings	C2	#4	8	12'-3"	Curb (long)	C2	#4	8	21'-9"	Curb (long)	F22	#5	4	10'-5"	
					A35	#6	12	29'-0"		C3	#4	8	13'-1"	Curb (long)	C3	#4	8	23'-10"	Curb (long)	F23	#5	4	11'-3"	
										C4	#4	4	9'-5"		C4	#4	4	24'-5"	Curb (long)	F24	#5	4	12'-1"	
										C5	#4	4	22'-10"		C5	#4	4	24'-5"	Curb (long)	F25	#5	4	12'-5"	
										C6	#4	4	23'-1"	Curb (long)	C6	#4	4	24'-5"	Curb (long)	F26	#5	4	13'-3"	
										C7	#4	4	3'-10"		C7	#4	4	24'-5"	Curb (long)	F27	#5	4	13'-10"	
																				F28	#5	4	14'-5"	
																				F29	#5	4	15'-0"	
																				F30	#5	4	15'-7"	
																				F31	#5	4	16'-2"	
																				F32	#5	4	16'-9"	
																				F33	#5	4	17'-4"	
																				F34	#5	4	17'-11"	
																				F35	#5	4	18'-6"	
																				F36	#5	4	19'-1"	
																				F37	#5	4	19'-8"	
																				F38	#5	4	20'-3"	
																				F39	#5	4	20'-10"	
																				F40	#5	4	21'-5"	
																				F41	#5	4	22'-0"	
																				F42	#5	4	22'-7"	
																				F43	#5	4	23'-2"	
																				F44	#5	4	23'-8"	
																				F45	#5	4	24'-3"	
																				F46	#5	4	24'-10"	
																				F47	#5	4	25'-5"	
																				F48	#5	4	26'-0"	Slab, Trans. Ends

V.A.B.P.

NOTES

DIMENSIONS ARE TO C OF BARS.
ALL REINFORCING STEEL SHALL BE
INTERMEDIATE GRADE.

Note: As built revisions shown in
black ink. 9-24-64 E. Barnard

DESIGN - H.R.G.
TRACE - F.B.P.
CHECK - R.S. NOTED

BRIDGE NO.
SURVEY -
PLOT -

STATE HIGHWAY COMMISSION
BRIDGE DIVISION

INTERSTATE 95 OVER RIVER ROAD

IN THE TOWN OF

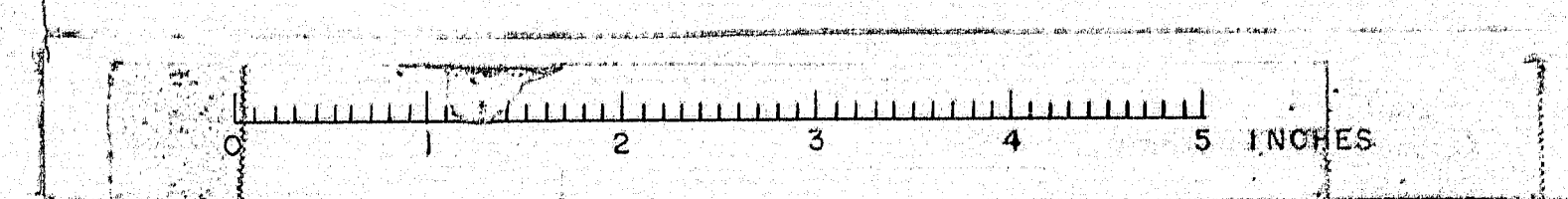
BENTON

KENNEBEC COUNTY

REINFORCING STEEL SCHEDULE SB

SHEET 28 OF 29 AUGUSTA, MAINE JUNE 1963

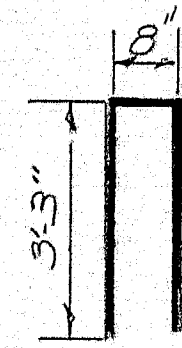
River Steel Schedules - River Rd 90-28



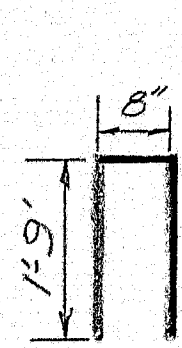
B. P. R. REG. NO.	STATE	PROJECT NUMBER	SHEET NO.	TOTAL SHEETS
1	MAINE	1-95-7(37)	22	22

PIER 3

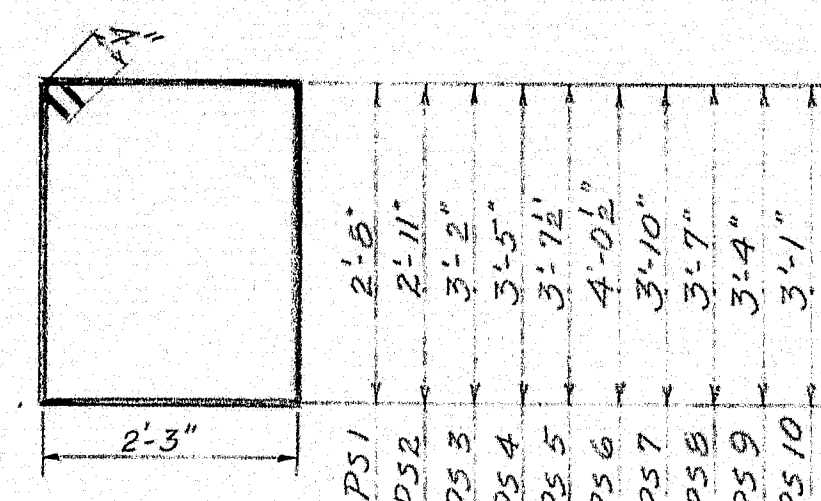
Hand-drawn diagram of a roof truss section. The diagram shows a horizontal line labeled "Hor. Leg" with a dimension of 3'-0". Below this line, there are two vertical dimensions: 2'-10" and 6'-0". To the right of these dimensions, there are two labels: A11 and A35. Further to the right, there is a vertical dimension of 7'-1" and a horizontal dimension of 4'-3". Below these dimensions, there are two labels: A34 and A12. At the top right, there is a dimension of 6'-2-5/8" and a label A36. At the bottom right, there is a dimension of 6'-2-5/8" and a label A37. A small square is drawn at the bottom right corner, with a dimension of 3'-0" and a label A38.



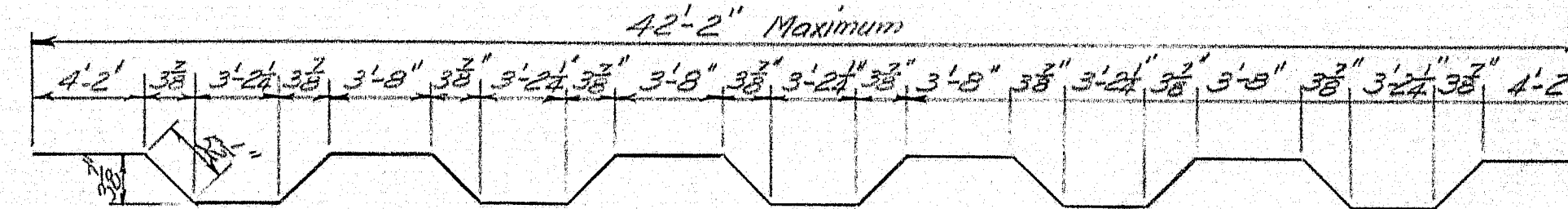
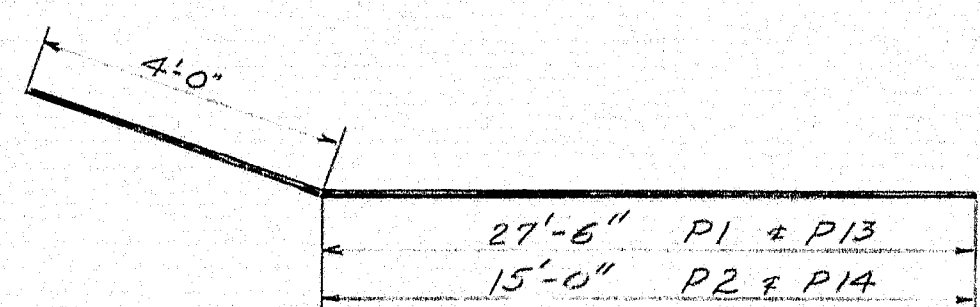
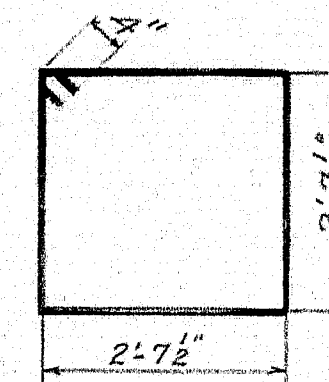
A-19



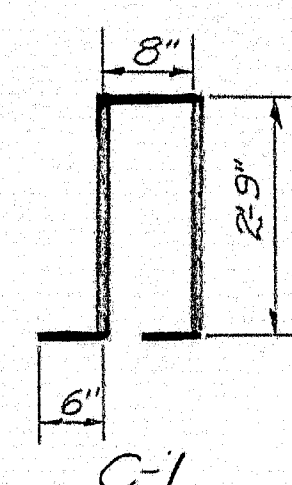
A23



PS 11



F-1



C-1

ABUTMENT #1				
MARK	SIZE	BENT	BARS	LOCATION
		NUMBER	LENGTH	
A 11	# 5	33	5'-10"	BREASTWALL
A 12	# 5	32	7'-2"	BREASTWALL
A 19	# 4	20	7'-2"	WINGS
A 23	# 4	6	4'-2"	END POSTS

ABUTMENT #2				
BENT BARS				
MARK	SIZE	NUMBER	LENGTH	LOCATION
A 19	#4	36	7'-2"	WINGS
A 23	#4	6	4'-2"	END POSTS
A 34	#5	33	10'-3"	BREASTWALL
A 35	#5	34	9'-0"	BREASTWALL
PS 11	#4	8	11'-2"	ADDITIONAL STRG. USED IN WIDENING SO. WING FOOTING

DIER #1				
BENT BARS				
MARK	SIZE	NUMBER	LENGTH	LOCATION
PS 1	#4	2	10'-6"	Pier Cap
PS 2	↑	2	11'-0"	
PS 3		2	11'-6"	
PS 4		2	12'-0"	
PS 5		44	12'-5"	
PS 6		38	13'-3"	
PS 7		2	12'-10"	

PIER #2				
BENT BARS				
MARK	SIZE	NUMBER	LENGTH	LOCATION
PS 1	#4	2	10'-6"	Pier Cap
PS 2		2	11'-0"	
PS 3		2	11'-6"	
PS 4		2	12'-0"	
PS 5		44	12'-5"	
PS 6		38	13'-3"	
PS 7		2	12'-10"	

SUPERSTRUCTURE				
BENT BARS				
MARK	SIZE	NUMBER	LENGTH	LOCATION
F 1	#5	149	43'-6"	SLAB
C 1	#4	238	7'-2"	CURBS
STRAIGHT BARS				
F 2	#5	298	42'-8"	SLAB

STRAIGHT BARS				
MARK	SIZE	NUMBER	LENGTH	LOCATION
F 48	#5	4	31'-0"	SLAB
F 49			31'-9"	
F 50			32'-5"	
F 51			33'-1"	
F 52			33'-9"	
F 53			34'-5"	
F 54			35'-1"	

		STRAIGHT		BARS		
A	1	#6	24	25'-0"	FOOTING (Speed)	
A 3	#6	98	5'-0"	"		
A 4	#5	66	3'-0"	FOOTING		
A 5	#	8	7'-6"	BEEHIVE WALL		
A 6	#	8	23'-9"	BEEHIVE WALL		
A 9	#	6	27'-0"	BARN WALL		
A 10	#	6	23'-9"	"		
A-13	#5	33	3'-0"	"		
A-15	#6	31	3'-6"	BACK WALL		
A-16	#6	8	11'-8"	SOUTH WING		
A 17	#5	40	2'-4"	BOTH WINGS		
A 18	#	20	7'-6"	SOUTH WING		
A 20	#	8	11'-0"	SOUTH WING		
A 21	#5	14	9'-8"	BOTH WINGS		
A 22	#4	4	2'-2"	END POSTS		
A 24	#4	8	9'-9"	BOTH WINGS		
A 25	#6	52	2'-6"	BOTH WINGS		
A 27	#6	8	12'-10"	NORTH WING		
A 28	#5	8	12'-0"	" "		
A 29	#5	20	6'-10"	NORTH WING		

		STRAIGHT		BARS	
A 22	#1		4	2'-2"	END POSTS
A 30	#6		32	26'-0"	FOOTING
A 32	#6		100	6'-6"	FOOTING
A 36	#5		33	5'-0"	BACKWALL
A 37			12	27'-4"	BREASTWALL
A 38			12	23'-6"	BREASTWALL
A 39			6	25'-0"	BACKWALL
A 40	#5		6	26'-0"	BACKWALL
A 41	#6		8	20'-11"	FULTING - SOUTH WING
A 42	#6		6	8'-0"	SOUTH WING
A 43	#6		8	15'-0"	NORTH WINGS
A 44	#5		8	17'-6"	BOTH WINGS
A 45	#4		8	17'-6"	" "
A46	#7		6	6'-0"	NORTH WING
A 47	#5		36	11'-0"	SOUTH WING
A48	#8		8	6'-0"	BREASTWALL
A 49	#5		6	6'-3"	NORTH WING
A 50	#7		14	10'-0"	BOTH WINGS
A 52	#7		7	18'-0"	NORTH WINGS
A 53	#5		36	10'-3"	NORTH WING
A 56			7	19'-11"	SOUTH WING
A 57	#5		8	17'-6"	BOTH WINGS
A 58	#6		8	20'-3"	NORTH WING FOOTING
A 60	#5		7	15'-11"	NORTH WING
A25	#6		86	2'-6"	FOOTINGS - BOTH WINGS
A4	#5		67	3'-0"	FOOTING
A17	#5		81	2'-4"	BOTH WINGS
A15	#6		32	3'-6"	BACKWALL
A61	#5		3	7'-0"	SOUTH WING
A62	#7		7	10'-0"	" "
A63	#7		8	6'-0"	BREASTWALL

PS 8	2	12'-4"	
PS 9	2	11'-10"	
PS 10	2	11'-4"	Pier Cap
PS 11	#4	9'04	Columns
P 1	#8	4	Pier Cap
P 2	#8	4	
P 13	#7	3	
P 14	#7	3	Pier Cap
STRAIGHT BARS			
P 3	#6	8	Pier Cap (Sides)
P 5	#8	10	25'-0" " (Top)
P 6	#9	4	25'-0" " "
P 11	#6	48	5'-6" Footings
D 3	#9	16	12'-8" Dowels (Fib)
D 4	#9	32	13'-8" "
D 5	#9	16	14'-8" "
P 15	#8	64	22'-0" Columns

P 1		2	12'-4"	
P 5 B		2	12'-4"	
P 5 B		2	11'-10"	
P 5 10		2	11'-4"	Pier Cap
P 5 11	#4	65	11'-2"	Columns
P 1	#8	4	31'-6"	Pier Cap
P 2	#8	4	19'-0"	
P 13	#7	3	31'-6"	
P 14	#7	3	19'-0"	Pier Cap
STRAIGHT BARS				
P 3	#6	8	25'-0"	Pier Cap (Girder)
P 5	#8	10	25'-0"	- (Top)
P 6	#9	4	25'-0"	"
P 11	#6	48	5'-6"	Footings
D 1	#8	64	8'-0"	Dowels (Ftgs.)
P 18	#8	16	19'-4"	Columns
P 19	#8	16	18'-4"	"
P 20	#8	16	17'-4"	"
P 21	#8	16	16'-4"	"

F	23	24	25
F 5	336	26'- 0"	
F 4	88	17'- 0"	
F 5	6	1'- 8"	
F 6	4	2'- 4"	
F 7		3'- 0"	
F 8		3'- 8"	
F 9		4'- 4"	
F 10		5'- 1"	
F 11		5'- 9"	
F 12		6'- 5"	
F 13		7'- 1"	
F 14		7'- 10"	
F 15		8'- 6"	
F 16		9'- 2"	
F 17		9'- 10"	
F 18		10'- 6"	
F 19		11'- 2"	
F 20		11'- 11"	
F 21		12'- 7"	
F 22		13'- 3"	
F 23		13'- 11"	
F 24		14'- 7"	
F 25		15'- 4"	
F 26		16'- 0"	
F 27		16'- 8"	
F 28		17'- 4"	
F 29		18'- 1"	
F 30		18'- 9"	
F 31		19'- 5"	
F 32		20'- 1"	
F 33		20'- 9"	
F 34		21'- 5"	
F 35		22'- 2"	

F 54			35'-10"	
F 56			36'-6"	
F 57			37'-2"	
F 58			37'-10"	
F 59			38'-7"	
F 60			39'-3"	
F 61			39'-11"	
F 62	# 5	4	40'-7"	SLAB
C 2	# 4	8	14'-11"	CURB
C 3		4	19'-11"	
C 4			23'-10"	
C 5			23'-3"	
C 6			21'-7"	
C 7		4	6'-11"	
C 8		8	21'-3"	
C 9		8	21'-0"	
C 10		4	6'-8"	
C 11			21'-2"	
C 12			24'-1"	
C 13			8'-9"	
C 14		4	19'-5"	
C 15	# 4	8	15'-3"	CURB
F 63	# 4	270	26'-9"	SLAB
v N.W.T.				
APPROACH SLABS				
A3 1	# 6	310	14'-8"	(Long End slabs)
A3 4	" 4	36	23'-2"	(7' from A"1) spliced
A3 5	" 4	36	25'-8"	(" " A"2) spliced
V.A.B.P				

V.A.B.P.

V.T.H.K.

NOTES

DIMENSIONS ARE TO C OF BARS.
ALL REINFORCING STEEL SHALL BE
INTERMEDIATE GRADE.

Note: As built revisions shown in
black ink. 9-24-64 E. Barnard.

K. N. W. T.

DESIGN- AJR	DET- JFM
RACE- JFM	
CHECK- <i>No Notes</i>	

STATE HIGHWAY COMMISSION
BRIDGE DIVISION
INTERSTATE 95 OVER RIVER ROAD
IN THE TOWN OF
BENTON
KENNEBEC COUNTY
REINFORCING STEEL SCHEDULE NB
SHEET 29 OF 29 AUGUSTA MAINE JUNE 1963

